

**City Council Meeting
August 1st, 2019
MUSCO TIF Request Memo**

MUSCO is many things to Muscatine and is encompassed in the long standing commitment to Muscatine. MUSCO is the premier large area lighting company and the Muscatine community is grateful to have them here in Muscatine. They are working to continue their commitment to Muscatine by building a 63,000 square foot, \$7.1 million dollar addition to their facility in Muscatine. The expanded facility will result in the addition of about 50 new Team members. MUSCO has requested Tax Increment Financing (TIF) to help support their project. The details of their project and request are below.

- Expansion of MUSCO facility to facilitate new technology production and increase in product demand.
- Creation of 40-50 new quality jobs
- \$7.1 million dollar expansion project with an estimated incremental valuation increase of \$3,500,00.00
- 63,000 sqft addition
- Requesting consent from Muscatine City Council to provide Tax Increment Financing (TIF) to MUSCO for the expansion to the MUSCO facility located at 2107 Stewart Road.
 - Asking for a 50% rebate for 10 years

Additional Information:

- TIF Calculation – City of Muscatine Finance Department
- MUSCO Project Site Plan



Adam J. Thompson
Director of Economic Development
563.263.8895

Proposed New Development: PREL. WORKSHEET MUSCO

Assumes All Commercial and/or Industrial Development at current 90% rollback

Date Prepared: 7-29-19

Parcel

1310376001

Valuations

Estimated Value After Improvement	\$	10,213,470	
Frozen Base		6,713,470	
Increment Estimates	\$	3,500,000	(100% Value before rollback)
Tax Rate for Increment (Per \$1,000) (FY 19 Rate for Increment)	\$	32.64978	(FY 19 Rate)

If 100% Rebate

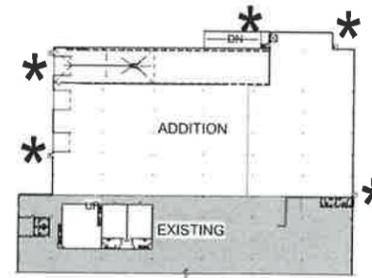
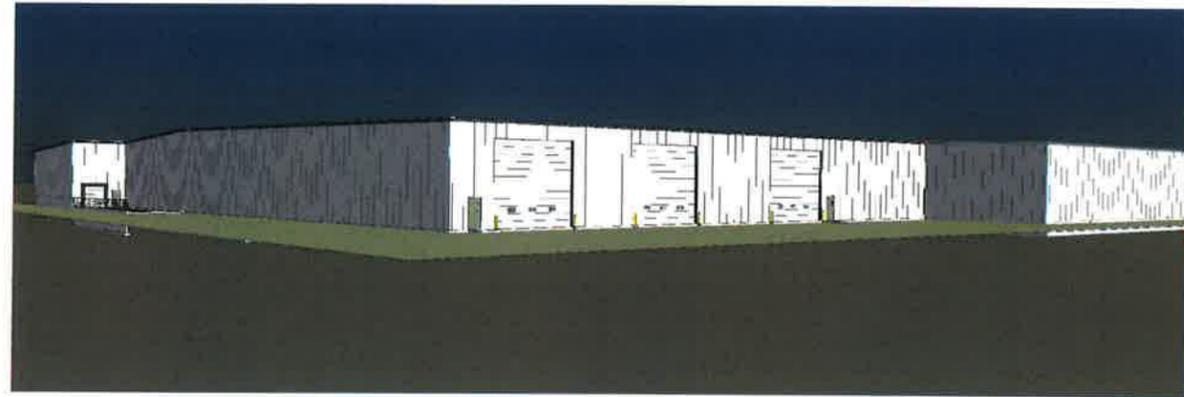
Incremental Taxes (Total Annual Estimate)	\$	114,274.23	(Before rollback)
Rebate Percentage (See Actuals Below)		100.00%	
Rebate (At 100% - See Actuals Below)	\$	114,274.23	(Before rollback)

Assumptions for Preliminary Rebate Calculation (Estimate):

1. Assumes project is commercial or industrial and the rollback remains at 90%.
2. Assumes FY 19 incremental tax rate for all years; actual rates will vary.
3. Assumes increment is \$3,500,000 at 100% valuation (actual incremental valuation may differ from this estimate).

Year	Estimated Commercial or Industrial Rebates					
	Increment at 100% of Assessed Value	Estimated TIF Tax Rate (per\$1,000)	Commercial/Ind. Rollback Factor	Rebate Percentage	Estimated Annual Rebate	Cumulative TIF Totals
1	\$ 3,500,000	\$ 32.64978	90.00%	50.00%	\$ 51,423	\$ 51,423
2	3,500,000	32.64978	90.00%	50.00%	51,423	102,846
3	3,500,000	32.64978	90.00%	50.00%	51,423	154,269
4	3,500,000	32.64978	90.00%	50.00%	51,423	205,692
5	3,500,000	32.64978	90.00%	50.00%	51,423	257,115
6	3,500,000	32.64978	90.00%	50.00%	51,423	308,538
7	3,500,000	32.64978	90.00%	50.00%	51,423	359,961
8	3,500,000	32.64978	90.00%	50.00%	51,423	411,384
9	3,500,000	32.64978	90.00%	50.00%	51,423	462,807
10	3,500,000	32.64978	90.00%	50.00%	51,423	514,230
					\$ 514,230	

MUSCO LIGHTING BUILDING ADDITION



KEY PLAN
1" = 100'-0"

* = EXIT



SCOPE OF WORK:
83,000 SF. BUILDING ADDITION FOR ASSEMBLY & STORAGE OF LIGHTING TOWERS

CODE DATA 2015 IBC

EXISTING BUILDING FOOTPRINT :	220,000 SF
BUILDING ADDITION FOOTPRINT :	63,000 SF
TOTAL BUILDING FOOTPRINT :	283,000 SF
OCCUPANCY GROUPS : (TABLE 308.2 & 304.1)	GROUP F-1 - MANUFACTURING/WAREHOUSE FOR ADDITION GROUP B - OFFICE/EXISTING
OCCUPANCY SEPARATION : (TABLE 508.4)	NOT REQUIRED
CONSTRUCTION TYPE :	TYPE IIB - PRE-ENGINEERED STEEL BUILDING 30 P.S.F. ROOF SNOWLOAD 100 P.S.F. LIVE LOAD MEZZANINE FLOOR 90 M.P.H. WIND LOAD
ALLOWABLE BUILDING SIZE : TABLE 506.2	62,000 SF
ALLOWABLE SIZE INCREASE : TABLE 507.4	UNLIMITED AREA, BUILDING IS FULLY FIRE SPRINKLED AND 60 FT OPEN YARDS ON 3 SIDES (CITY FIRE DEPARTMENT HAS APPROVED 40 FT ON WEST SIDE)
AUTOMATIC FIRE SPRINKLER :	PROVIDED THRU-OUT EXISTING & ADDITION PER (SEC 903.2.4)
OCCUPANT LOAD FOR ADDITION : (TABLE 1004.1.2)	PRODUCTION FACILITY AREA SF/200 TOTAL OCCUPANT LOAD OF ADDITION 315
EXITS REQUIRED : (SEC 1006.3.1, SEC. 1007.1.2)	PRODUCTIONWAREHOUSE FOUR (4) 3'-0" WIDE EXITS REQ'D
MAX TRAVEL DISTANCE : (TABLE 1017.2)	250 FT. FOR FIRE SPRINKLED BUILDING THIS OCCUPANCY ALL EXISTING DOORS KEPT OR RELOCATED AT ADDITION
EXITS PROVIDED :	PRODUCTIONWAREHOUSE - FIVE (5) 3'-0" WIDE EXITS PROVIDED - SEE FLOOR PLAN / KEY PLAN
EMERGENCY & EXIT LIGHTS : SEC. 1008.2	SEE REFLECTED CEILING PLAN FOR LOCATIONS. PROVIDE MIN. 2 FT CANDLES @ EXIT PATHWAY - VERIFY WITH FIRE DEPT.
FIRE EXTINGUISHERS :	4A 10BC - 10 LB. IN PRODUCTION/WAREHOUSE - SEE FLOOR PLAN FOR LOCATIONS - VERIFY WITH FIRE DEPT.
HANDICAP ACCESSIBLE BUILDING : (SEC. 1103.1.3 & SEC. 11.3.1.5)	YES
FLAMMABLE MATERIALS	FLAMMABLE MATERIALS SHALL BE STORED IN FIRE RATED CABINETS PROVIDED BY OWNER. INFORMATION ON RATINGS OF THESE CABINETS SHALL BE PROVIDED TO OSKALOOSA FIRE DEPT. PRIOR TO PRODUCTION START-UP
DRINKING FOUNTAIN	BOTTLED WATER AND/OR WATER COOLERS PROVIDED BY OWNER IN LIEU OF DRINKING FOUNTAIN

GENERAL NOTES

1. PROVIDE AND INSTALL ALL WORK AND MATERIALS AS REQUIRED BY THE DRAWINGS AND SPECIFICATIONS - IN FULL ACCORDANCE WITH ALL CODES AND ORDINANCES.
2. INFORM ARCHITECT OF ANY DIMENSIONAL DISCREPANCIES PRIOR TO CONSTRUCTION.
3. DRAWINGS SHOULD NOT BE SCALED - DIMENSIONS GOVERN. LARGE SCALE DRAWINGS GOVERN OVER SMALL SCALE DRAWINGS.
4. INFORM ARCHITECT OF ANY EQUIPMENT, PRODUCT, OR MATERIAL SUBSTITUTES PRIOR TO FABRICATION AND INSTALLATION.
5. REFER TO STRUCTURAL DRAWINGS FOR ALL STRUCTURAL SIZES, SHAPES AND DESIGN INFORMATION.
6. DESIGNATED TRASH AND RECYCLING AREA SHALL BE PROVIDED WITHIN THE BUILDING.
7. PROVIDE MOISTURE RESISTANT GYP. BD. AT ALL TOILET ROOM WET WALLS OR WHERE CERAMIC TILE OR F.R.P. IS INDICATED. THIS INCLUDES ALL JANITOR CLOSETS.
8. PROVIDE CONTROL JOINTS IN GYP. BD. AS RECOMMENDED BY MANUFACTURER.
9. ALL GLAZING SUBJECT TO HUMAN IMPACT SHALL BE DESIGNED AND SUPPORTED TO WITHSTAND LOADS IMPOSED BY CURRENT BUILDING CODE.
10. THE TYPICAL DIMENSIONS FROM THE HINGE SIDE OF A DOOR OPENING TO THE FACE OF ADJACENT PERPENDICULAR WALL IS 5". UNLESS NOTED OTHERWISE ALL THRESHOLDS TO BE 1/2" HIGH MAX. PER ADA.
11. ANY DOOR CLOSER SHALL BE MOUNTED ON THE TENANT SIDE OF ENTRY DOORS.
12. INTERIOR FINISHES SHALL COMPLY WITH THE FLAME SPREAD CLASSIFICATIONS AS REQUIRED BY CURRENT BUILDING CODE.
13. EXIT SIGNS SHALL BE PROVIDED AS REQUIRED. SEE ELECTRICAL DRAWINGS.
14. ARCHITECT TO APPROVE LOCATION OF EMERGENCY EXIT SIGNS, SPRINKLER HEADS, AND MECHANICAL GRILLES PRIOR TO INSTALLATION.
15. FIRE EXTINGUISHERS SHALL BE PROVIDED. VERIFY LOCATIONS WITH LOCAL FIRE DEPARTMENT.
16. SPRINKLER SYSTEM SHALL BE E.S.F.R. INSTALLED IN ACCORDANCE WITH N.F.P.A. NO. 13 & F.M. GLOBAL. SHOP DRAWINGS FOR FIRE PROTECTION SYSTEMS SHALL BE SUBMITTED BY FIRE PROTECTION SUBCONTRACTOR TO THE G.C. AND THE FIRE MARSHALL FOR APPROVAL.
- 17.

I HEREBY CERTIFY THAT THE PORTION OF THIS TECHNICAL SUBMISSION DESCRIBED BELOW WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND RESPONSIBLE CHARGE. I AM A DULY REGISTERED ARCHITECT UNDER THE LAWS OF THE STATE OF IOWA.

DATE: 09/21/14
JOB NO.: 17V-0012

PAGES OR SHEETS COVERED BY THIS SEAL:
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LOCATION MAP



CONTACTS

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INDEX OF DRAWINGS

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ARCHITECTURAL	A1.0 / FLOOR PLAN A1.1 / ENLARGED PLANS A3.0 / BUILDING ELEVATIONS A4.1 / WALL SECTIONS A4.2 / WALL SECTIONS A4.3 / WALL SECTIONS A8.0 / DOOR SCHEDULE and ADA MNTG HEIGHTS

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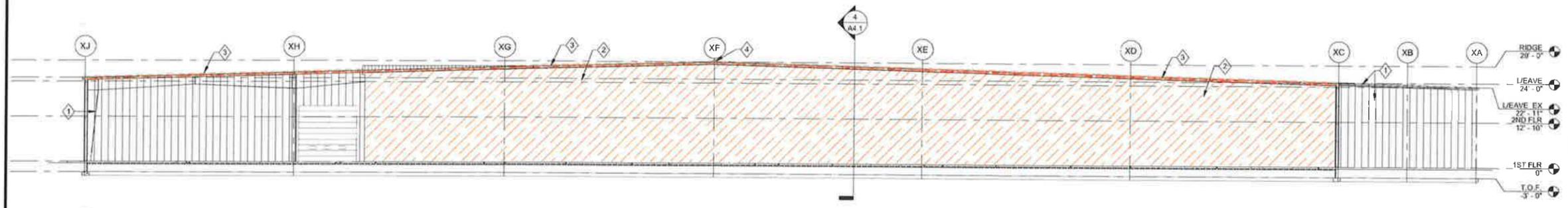
MUSCO LIGHTING BUILDING ADDITION
2107 Stewart Rd
Muscatine, IA 52761

REVIEW SET

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DATE 09-21-14
JOB NO. 17V-0012

COVER SHEET

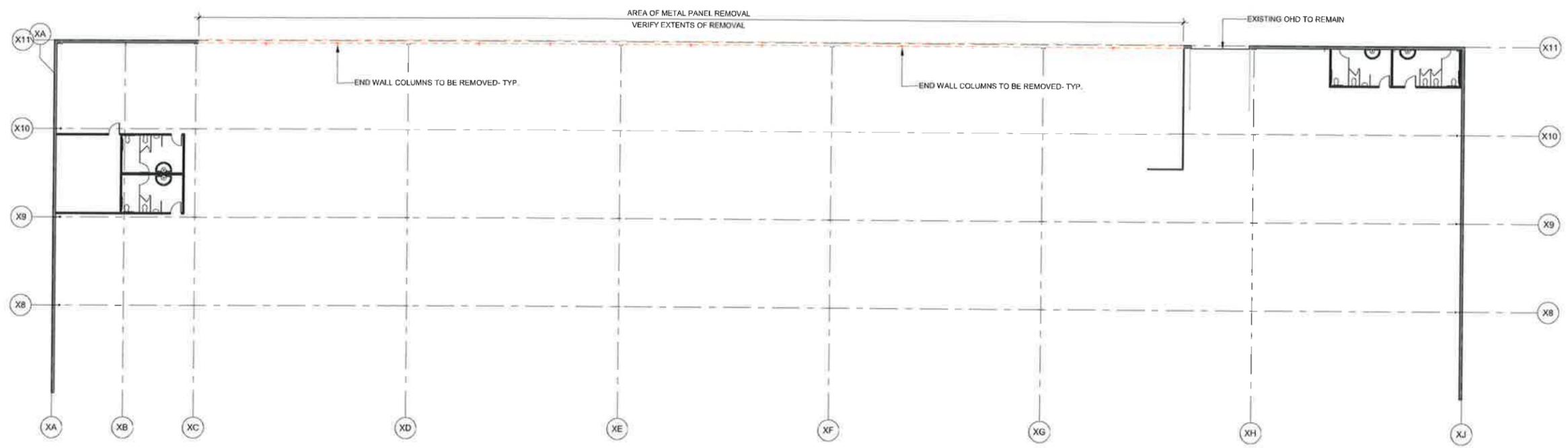
SHEET
COVER



EXT. ELEV. DEMO NOTES

- 1 EXISTING SIDING AND / OR TRIMS TO REMAIN
- 2 DEMO EXISTING METAL WALL PANELS- VERIFY AMOUNT TO REMOVE WITH OWNER
- 3 DEMO EXISTING FASCIA TRIM
- 4 PREPARE EXISTING RIDGE CAP FOR EXTENSION

1 NORTH DEMO ELEVATION
AD1.0 1/16" = 1'-0"



2 DEMO PLAN
AD1.0 1/16" = 1'-0"

DEMOLITION GENERAL NOTES

1. CONTRACTOR TO VERIFY DIMENSIONS AND CONDITIONS SHOWN ON THESE DRAWINGS. ANY OMISSIONS, DISCREPANCIES, OR CONFLICTS MUST BE REPORTED TO THE ARCHITECT IMMEDIATELY PRIOR TO BIDDING.
2. CONTRACTOR SHALL PROTECT ALL EXISTING CONSTRUCTION TO REMAIN.
3. FLOORS TO BE PREPPED AND CLEANED TO RECEIVE NEW FINISHES, TYPICAL.
4. REMOVE EXISTING CONSTRUCTION AS SHOWN.
5. CONTRACTOR SHALL PATCH ALL SURFACES DISTURBED BY DEMOLITION.
6. CONTRACTOR SHALL PROVIDE TEMPORARY ENCLOSURES AT WINDOW AND DOOR OPENINGS AS REQUIRED.
7. SALVAGE EXISTING DOOR, LATCH SETS. VERIFY FOR REUSE IN AREA OF NEW CONSTRUCTION.
8. SPECIAL CARE SHOULD BE TAKEN WITH THE HANDLING AND STORAGE OF PLUMBING FIXTURES TO BE SALVAGED FOR REUSE.
9. THE EXISTING ELECTRICAL INSTALLATION AS SHOWN ON THE DRAWINGS MAY NOT BE COMPLETELY ACCURATE IN ALL RESPECTS WITH REGARD TO QUANTITIES, LOCATIONS, ETC. AND IS SHOWN PRIMARILY TO GENERALLY ILLUSTRATE THE DEGREE OF DEMOLITION WORK INVOLVED. DEMOLITION WORK SHALL BE INCLUSIVE IN AREA SHOWN.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DEMOLITION OF EXISTING MATERIALS AS SHOWN AS NECESSARY TO ACCOMPLISH THE WORK INDICATED ON THE DRAWINGS. MATERIALS SHOWN TO BE REMOVED AND NOT REUSED SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE REMOVED FROM THE SITE UNLESS NOTED OTHERWISE.
11. WIRING IN THE EXISTING BUILDING SHALL REMAIN AS IS EXCEPT AS INDICATED ON THE DRAWINGS.
12. WHERE EXISTING WALLS, CEILINGS, FLOORS, FIXTURES, DEVICES, EQUIPMENT OR OTHER OUTLETS ARE INDICATED TO BE REMOVED THE CONTRACTOR SHALL EXTEND EXISTING CIRCUITING, REMOVE UNUSED ACCESSIBLE CONDUIT AND INSTALL NEW PLATES WITH BLANK GANGS AS REQUIRED ON EXISTING OUTLET BOXES, MAINTAIN CIRCUIT CONTINUITY TO EXISTING RECEPTACLES AND LIGHTING WHICH IS TO REMAIN.
13. THE CONTRACTOR SHALL DISCONNECT AND REMOVE UNUSED CONDUIT AND OTHER RACEWAYS NO LONGER IN USE.
14. REMOVAL OF DISCHARGE DUCTWORK ON ALL VAV BOXES. THIS CAN BE CRUCIAL. THERE IS POTENTIAL FOR EXISTING DUCTWORK LEFT, TO BE INADEQUATE SIZING FOR THE NEW SPACE LAYOUTS. INADEQUATELY SIZED DUCTS COULD CAUSE UNWANTED PRESSURE DROP AND MAY NOT DELIVER EXPECTED QUANTITIES OF AIR/VENTILATION TO SPACES. DUCTS MAY NOT BE PROPERLY SEALED AND MAY NOT DELIVER EXPECTED QUANTITIES OF AIR/VENTILATION TO SPACES, ETC.
15. EXISTING FIRE SPRINKLER SYSTEM TO REMAIN. CONTRACTOR TO MODIFY SPRINKLER SYSTEM IN AREAS WHERE CEILING IS REMOVED SO AS TO COMPLY WITH LOCAL FIRE CODES.
16. DEMOLISH THE FOLLOWING ITEMS: PARTITIONS, DOORS, CASEWORK, ETC. SHOWN DASHED & ANY CEILINGS & FLOOR FINISHES INDICATED ON DRAWINGS.

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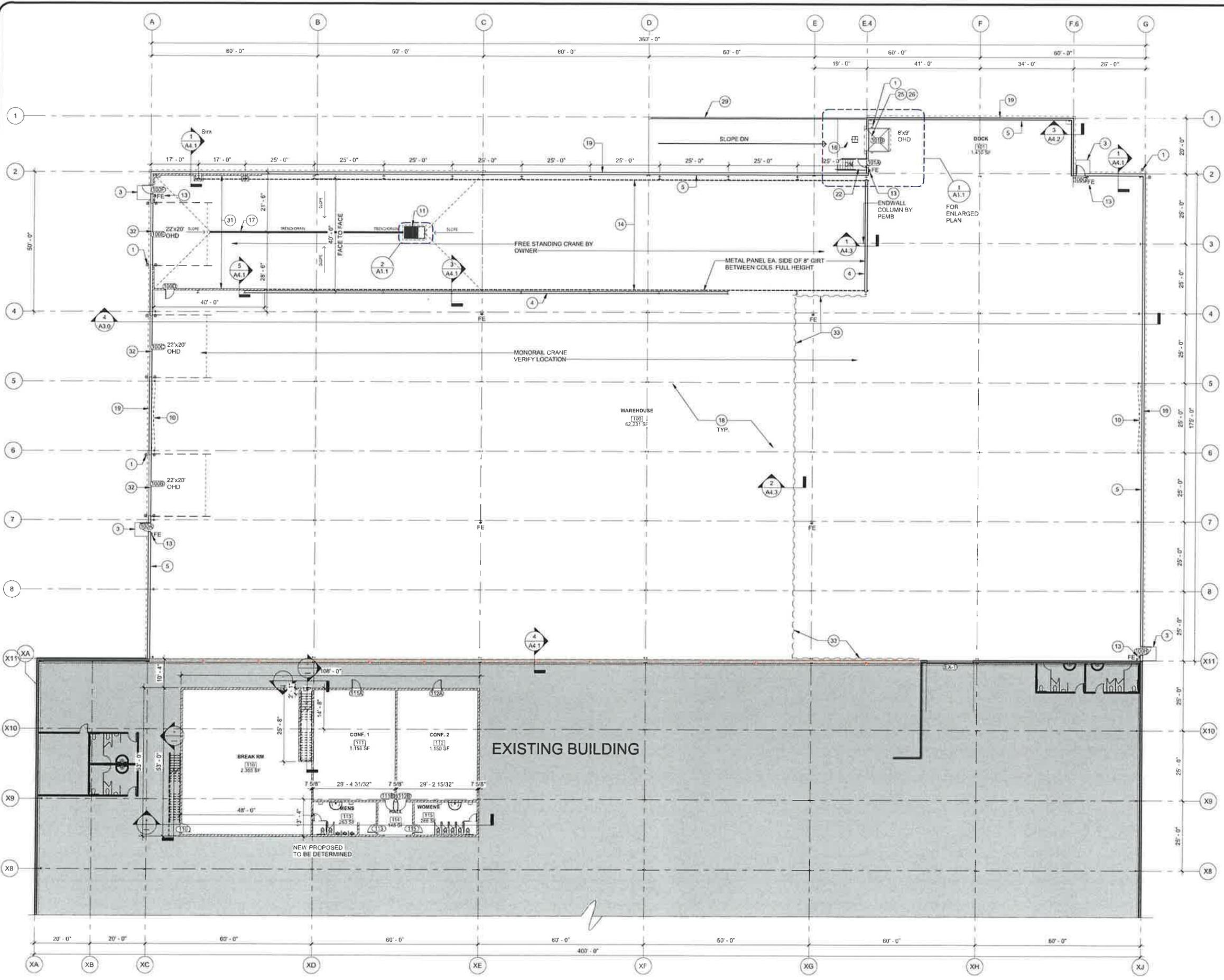
MUSCO LIGHTING BUILDING ADDITION
2107 Stewart Rd
Muscatine, IA 52761

REVIEW SET

DRAWN: C-EGGSEZ	CHECKED: C-EGGSEZ
AUTHOR: C-EGGSEZ	CHECKED: C-EGGSEZ
DATE: 05-21-14	JOB NO: D-1-0012

DEMOLITION PLAN

SHEET
AD1.0



GENERAL NOTES

- REFER TO STRUCTURAL DRAWINGS FOR SLAB ON GRADE CONSTRUCTION, CONTROL AND ISOLATION JOINTS, COORDINATE UNDERGROUND AND UNDER SLAB UTILITIES WITH RELEVANT TRADES PRIOR TO SLAB POUR. COORDINATE ALL SLEEVES THROUGH/UNDER FOOTING AND FOUNDATION W/STRUCTURAL ENGINEER. ALL WALLS TO ROOF DECK TO BE CONSTRUCTED TO ACCOMMODATE DEFLECTION OF ROOF STRUCTURE.
- REFER TO STRUCTURAL ENGINEERING DRAWINGS FOR ADDITIONAL INFORMATION PERTAINING TO STRUCTURAL COMPONENT SIZES, LOCATIONS, CONFIGURATIONS, AND CAPACITIES.
- PROVIDE WALL MOUNTED FIRE EXTINGUISHERS (FE) OF SIZE AND TYPE AND LOCATIONS AS REQUIRED BY THE FIRE MARSHAL.
- ALL EQUIPMENT SHOWN FOR REFERENCE ONLY. REFER TO MECHANICAL AND ELECTRICAL DRAWINGS FOR EXACT EQUIPMENT, SIZE AND LOCATION.

DIMENSIONS:

- SUBCONTRACTOR TO VERIFY DIMENSIONS AND CONDITIONS SHOWN ON THESE DRAWINGS. ANY OMISSIONS, DISCREPANCIES, OR CONFLICTS MUST BE REPORTED TO THE ARCHITECT IMMEDIATELY. PRIOR TO BIDDING.
- DIMENSIONS ARE AT 3'-0" ABOVE FINISHED FLOOR AND FROM FACE OF FINISHED PARTITION UNLESS NOTED OTHERWISE.
- DRAWINGS GOVERN OVER SMALL SCALE DRAWINGS.
- DIMENSIONS AT EXTERIOR WALLS ARE TAKEN TO EXTERIOR FACE OF CONCRETE WALL OR TO EXTERIOR SIDE OF SHEATHING UNLESS NOTED OTHERWISE.
- DIMENSIONS AT INTERIOR WALLS ARE TAKEN TO FACE OF GYP. UNLESS NOTED OTHERWISE.

PLAN KEYNOTES

- 6" CONCRETE FILLED BOLLARD (TYPE 1) AT EXTERIOR OR 6" BOLTED DOWN BOLLARD (TYPE 3) AT INTERIOR
- HOSE BIB - SEE PLUMBING PLANS
- CONCRETE STOOP WITH FROST FOOTING - SEE STRUCTURAL
- PARTITION WALL CONSISTING OF WALL GIRTS WITH LINER PANEL E.S., FULL HEIGHT
- 6'-0" HIGH LINER PANEL AROUND PERIMETER, TYP. MATCH LINER PANEL HEIGHTS IN EXISTING AREAS AS NEEDED
- CONDENSER - TYPE & LOCATION T.B.D. - SEE MECH. PLANS
- EXHAUST - TYPE & LOCATION T.B.D. - SEE MECH. PLANS
- ELECTRICAL PANELS - SEE ELEC. PLANS
- SPRINKLER RISER/FIRE PUMP - SEE SPRINKLER PLANS
- PRE-ENGINEERED X-BRACING IN BAY INDICATED - VERIFY W/PEMB
- MUD AND OIL SEPARATOR SUMP FOR THE TRENCH DRAINS - COORDINATE LOCATION WITH PLUMBING CONTRACTOR
- KNOX BOX - RELOCATE EXISTING - VERIFY LOCATION WITH F.D.
- FIRE EXTINGUISHER LOCATION - VERIFY LOCATIONS WITH F.D.
- CRANE RAILS AND COLUMNS BY OTHERS
- FLOOR DRAIN, TYP. - SEE PLUMBING PLANS
- DOCK AREA DRAIN
- 8" TRENCH DRAIN, TYP. SLOPE SURROUNDING CONCRETE SLAB TO DRAIN AS REQUIRED
- POLISHED CONCRETE FLOORS, TYP. SEE STRUCTURAL DRAWINGS FOR SLAB THICKNESS AND REINFORCING
- PRE-ENGINEERED METAL WALL PANELS, TO MATCH EXISTING, INSULATION AND BUILDING FRAMES
- 24"x24" MOP SINK - SEE PLUMBING PLANS. PROVIDE FRP SURROUND (48" HIGH) E.S., TYP.
- METAL STAIRS TO MEZZANINE LEVEL ABOVE - SEE STAIR PLANS & SECTIONS
- METAL STAIRS TO ENTRY/EXIT - SEE STAIR PLANS & SECTIONS
- LOCKERS BY OWNERS - VERIFY SIZE & LAYOUT
- VENDING MACHINES BY OWNER
- 8'x8' INSULATED DOCK DOOR, DOCK BUMPERS & SEALS, TYP. PROVIDE HYDRAULIC LEVELERS (10,000 LB. CAPACITY), VEHICLE RESTRAINTS & LOADING DOCK LIGHT COMMUNICATION SYSTEM
- LED DOCK LIGHT, TYP. - VERIFY TYPE TO BE USED W/OWNER. MOUNT 16'-0" A.F.F. OR AS REQUIRED
- PROVIDE "TORNADO" RATED DOORS AT STORM SHELTER OPENINGS. EF-3
- PROVIDE "TORNADO" RATED ROLLING SHUTTERS AT EXTERIOR & INTERIOR OPENINGS. EF-3 RATED
- 42" HIGH METAL GUARDRAIL, TYP.
- 42" HIGH METAL SPRINGLOADED SWING GATE, TYP.
- 10' TALL PAINTED CMU WALL W/ HORIZ TIES @ 16" O.C. AND #5 BARS @ 24" O.C. AND @ CORNERS IN GROUTED FULL CORES. BOND BEAM TOP COURSE W/ (2) #4
- INSULATED 22"x20" OHD W/ OPERATORS. SEE DOOR SCHEDULE
- 2' TALL SUSPENDED LINER PANEL DRAFT CURTAIN

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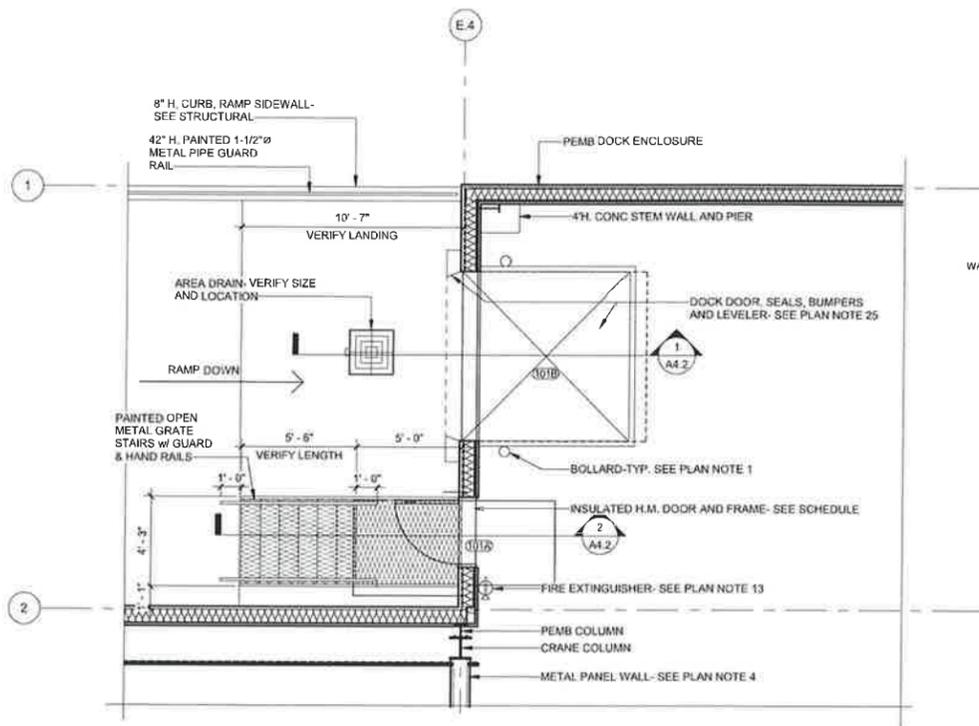
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FLOOR PLAN

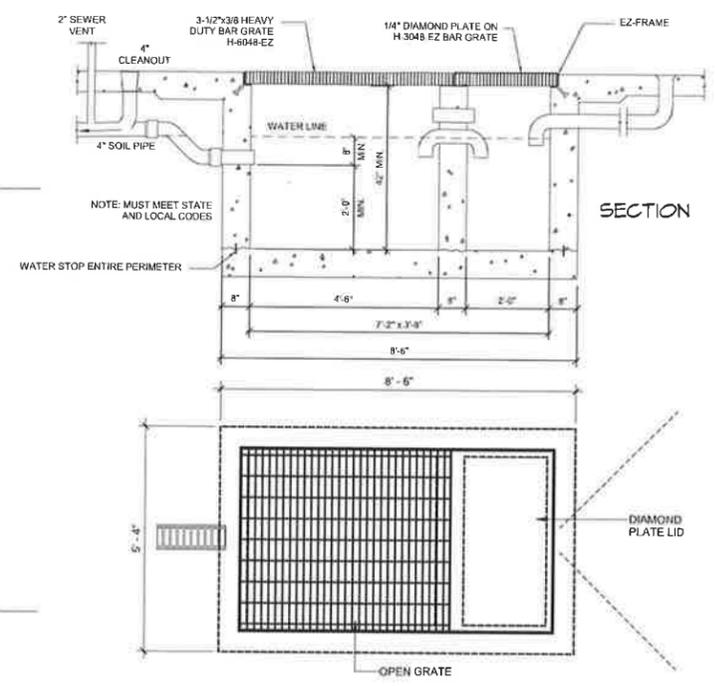
SHEET
A1.0

1 1ST FLR PLAN
 AT.0 1/16" = 1'-0"

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1 ENLARGED DOCK PLAN
A1.1 1/4" = 1'-0"



2 ENLARGED OIL SEPARATOR SUMP PLAN
A1.1 1/2" = 1'-0"

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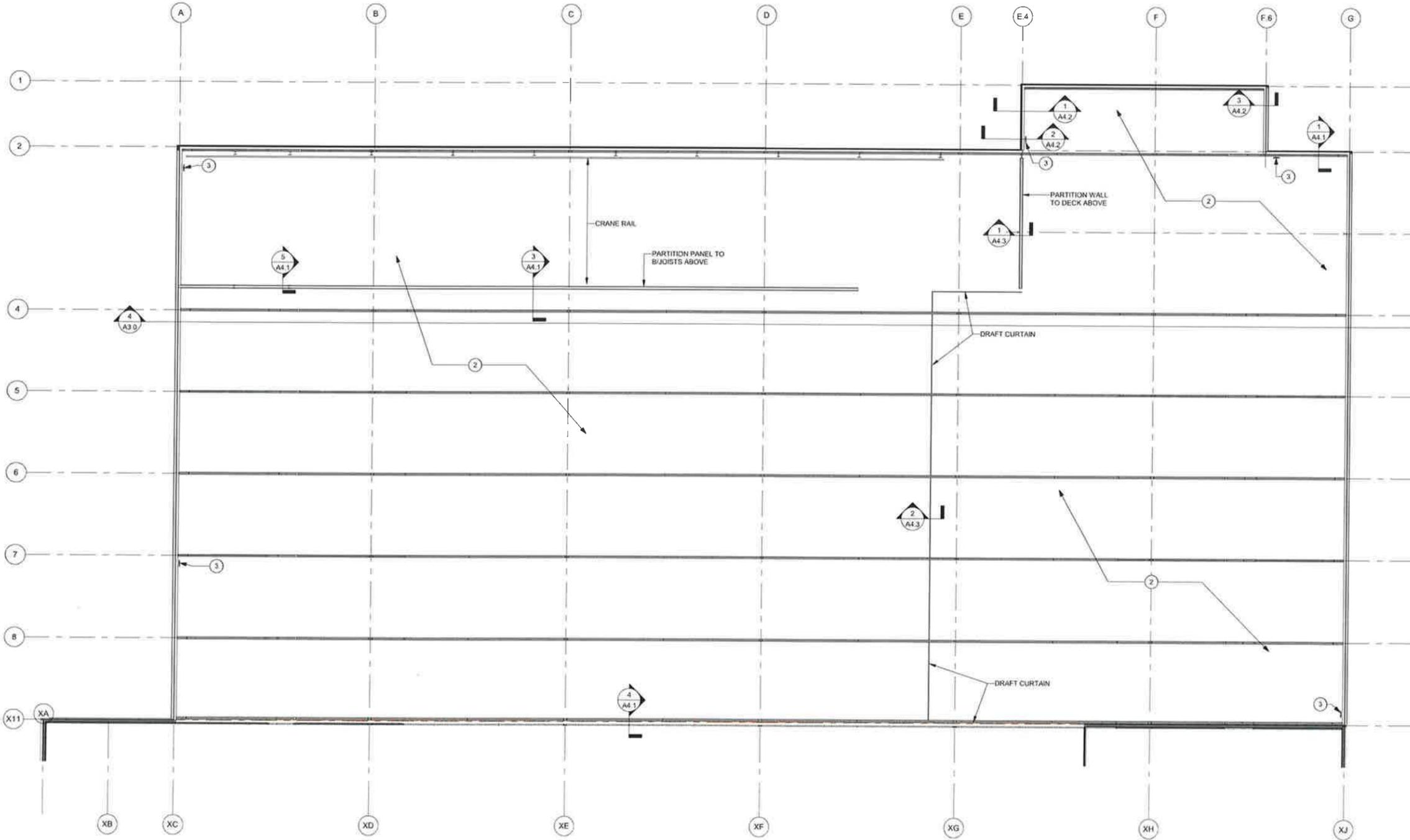
ENLARGED PLANS

SHEET
A1.1

- RCP KEYNOTES**
- ACCESS LADDER TO ROOF ABOVE - SEE SHEET A520
 - OPEN TO PRE-ENGINEERED STRUCTURE INSULATION SYSTEM ABOVE
 - EMERGENCY LIGHT AND EXIT SIGN- VERIFY LOCATIONS

- REFLECTED CEILING PLAN SYMBOLS**
- 2'X4' FLORESCENT LIGHT
 - RELOCATED OR NEW 2'X4' FLORESCENT LIGHT
 - S SPRINKLER HEADS
 - RS RELOCATED SPRINKLER HEADS
 - RECESSED DOWNLIGHT
 - TRACK LIGHT
 - ⊗ NEW EXIT LIGHT
 - ⊙ PENDANT LIGHT
 - INDIRECT/ DIRECT LIGHTING- BUILDING STANDARD
 - EMERGENCY LIGHT AND EXIT SIGN

- REFLECTED CEILING PLAN GENERAL NOTES**
- EXISTING CEILING GRID AND HEIGHT TO REMAIN AS SHOWN. PROVIDE NEW GRID WHERE EXISTING GRID IS DAMAGED OR MISSING.
 - THIS PLAN SHOWS ONLY FINAL LIGHT LOCATIONS AND DOES NOT DIFFERENTIATE BETWEEN EXISTING, RELOCATED, OR NEW FIXTURES. CONTRACTOR TO RELOCATE EXISTING FIXTURES AND PROVIDE NEW FIXTURES AS FIELD CONDITIONS REQUIRE. OPERABLE EXISTING LIGHT FIXTURES TO BE REUSED. REPLACE ANY EXISTING FIXTURES WHICH ARE DAMAGED. NEW FIXTURES AND LAMPS MUST MATCH EXISTING EXISTING SPRINKLERS (NOT SHOWN) TO REMAIN WHERE POSSIBLE. PROVIDE NEW AND RELOCATE EXISTING AS REQUIRED BY CODE.
 - LOCATE LIGHT FIXTURES AND OTHER CEILING ELEMENTS CENTERED IN THE TILE ON WHICH THEY OCCUR. NOTIFY ARCHITECT IF CONFLICTS ARISE.
 - REMOVE FILM COVER UPON FINAL CLEAN.
 - NEW LIGHT FIXTURES SHALL BE INSTALLED WITH PROTECTIVE FILM COVER INTACT.
 - WHEN SWITCHES ARE INSTALLED IN A GROUPING OF TWO OR MORE, TOGGLE (OR ROCKER) SWITCHES SHALL BE GANGED TOGETHER UNDER ONE (1) FACE PLATE. PROVIDE DIVIDER IN BACK BOX WHEN DUAL VOLTAGES ARE PRESENT.
 - INSTALL THERMOSTATS ALIGNED WITH LIGHT SWITCHES, AS CLOSE TO THE CORNER OF THE ROOM WITHOUT COMPROMISING ACCURACY OF THE READING. THERMOSTATS MUST NOT BE INSTALLED IN THE CENTER OF A WALL. CONTACT ARCHITECT IF CONFLICTS ARISE.
 - ENCLOSED ROOMS TO BE SWITCHED SEPARATELY FROM THE OPEN OFFICE AREA, UNLESS NOTED OTHERWISE.
 - PROVIDE BASE PANEL IN-FEED AS REQUIRED FOR FURNITURE SYSTEMS LOCATED ADJACENT TO WALLS.
 - PROVIDE POWER AS REQUIRED FOR NEW BREAK ROOM LAYOUT.



1 REFLECTED CEILING PLAN
A2.0 1/16" = 1'-0"

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VOICE 319.266-6383

MUSCO LIGHTING BUILDING ADDITION
2107 Stewart Rd
Muscatine, IA 52761

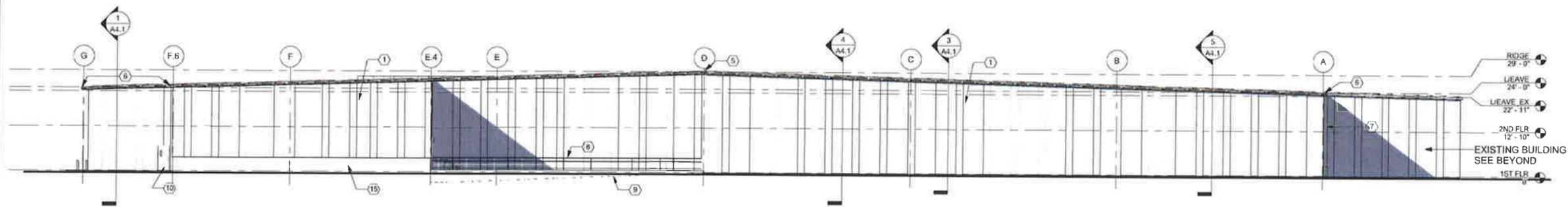
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Author	Checker
DATE	JOB NO.
09-21-14	D-1-0012

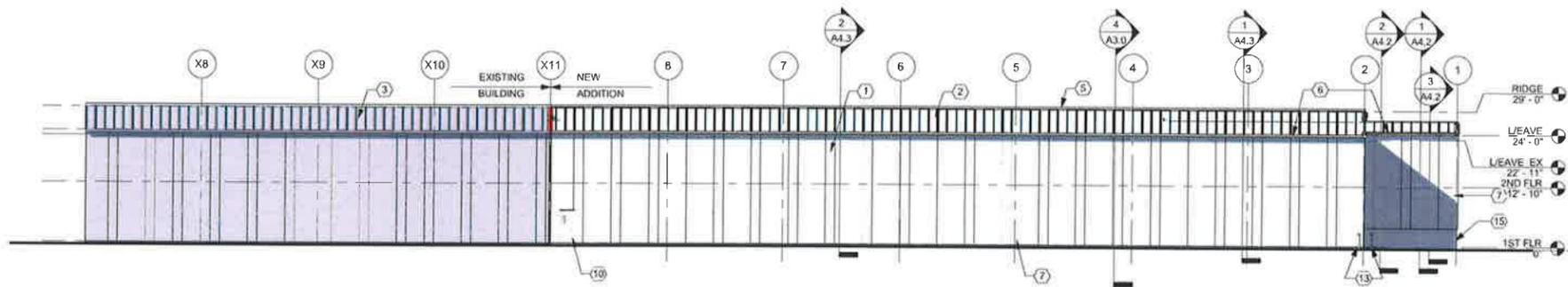
REFLECTED CEILING PLAN

SHEET
A2.0

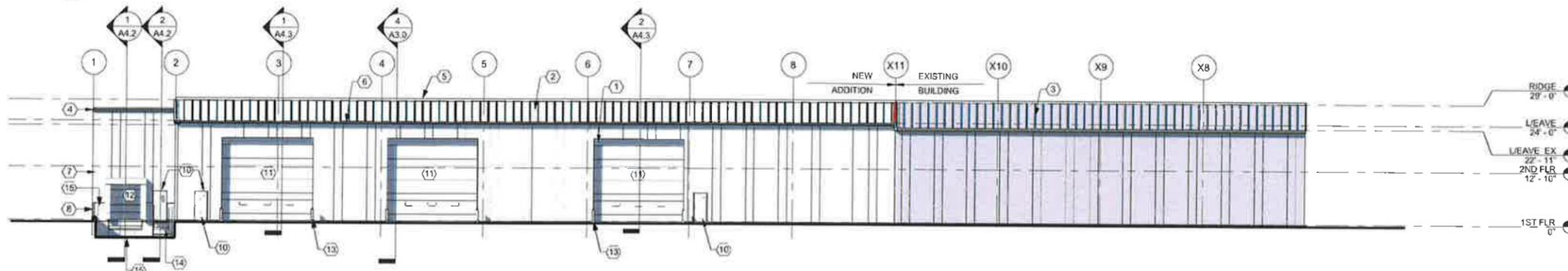
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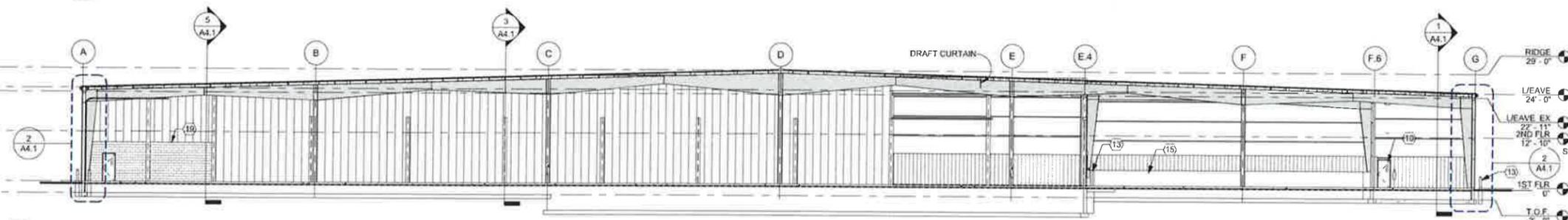
1 NORTH ELEVATION
A3.0 1/16" = 1'-0"



2 EAST ELEVATION
A3.0 1/16" = 1'-0"



3 WEST ELEVATION
A3.0 1/16" = 1'-0"



4 BUILDING SECTION
A3.0 1/16" = 1'-0"

CONTRACTOR SHALL FIELD VERIFY ALL HEIGHTS PRIOR TO BID- ANY DISCREPANCIES SHALL BE GIVEN TO ARCHITECT

EXTERIOR ELEVATION KEYNOTES

- 1 METAL WALL PANEL: 26 GA - 36" PANEL COVERAGE. PANEL TO MATCH EXISTING COLOR & PROFILE
- 2 METAL ROOF PANEL: NUCOR "CFR" STANDING SEAM ROOF - 24 GAUGE - 24" PANEL COVERAGE. COLOR: GALVALUME
- 3 EXISTING ROOF PANEL: NO CHANGE
- 4 METAL EAVE TRIM: MATCH EXISTING COLOR & PROFILE
- 5 METAL RIDGE CAP: NUCOR RIDGE CAP - 26 GAUGE. COLOR: GALVALUME
- 6 METAL GUTTER: NUCOR GUTTER - 26 GAUGE. COLOR: MATCH WALL PANELS
- 7 METAL DOWNSPOUT: NUCOR CORRUGATED 5" DOWNSPOUT. COLOR: MATCH WALL PANELS - VERIFY LOCATION. COORDINATE WITH CIVIL
- 8 1-1/2" Ø PAINTED METAL GUARD RAIL ON 8" CONCRETE RAISED RAMP RETAINING WALL
- 9 DOCK RAMP: SEE STRUCTURAL / CIVIL DRAWINGS
- 10 INSULATED HM DOOR & FRAME: NUCOR DOORS OR EQUAL - PAINTED TO MATCH ADJACENT METAL PANEL COLOR. PROVIDE INSULATED GLAZING WHERE SHOWN. SEE PLANS FOR SIZES
- 11 INSULATED METAL OVERHEAD SECTIONAL DOOR & FRAME: OVERHEAD DOOR - MODEL 515 (R-12 1/2 OR GREATER) OR APPROVED EQUAL. PROVIDE INSULATED GLAZING WHERE SHOWN. DOOR COLOR: WHITE, STANDARD. SEE PLANS FOR SIZES
- 12 INSULATED METAL 8'X9' DOCK DOORS, DOCK BUMPERS & SEAL. TYP. PROVIDE HYDRAULIC LEVELERS (10,000 LB. CAPACITY), VEHICLE RESTRAINTS & LOADING DOCK LIGHT COMMUNICATION SYSTEM. DOOR BY OVERHEAD DOOR - MODEL 515 (R-12 1/2 OR GREATER) OR APPROVED EQUAL. DOOR COLOR: WHITE, STANDARD. SEAL COLOR: DARK GRAY. SEE PLANS FOR DETAILS
- 13 6" CONCRETE FILLED EXTERIOR BOLLARD (TYPE 1) - SEE CIVIL DRAWINGS. PAINT SAFETY YELLOW, TYP.
- 14 METAL STAIRS TO ENTRY/EXIT - SEE STAIR PLANS & SECTIONS
- 15 EXPOSED CONCRETE FOUNDATION WALL, TYP.
- 16 EXISTING EXTERIOR SHOE BOX LIGHT. REUSE OR REPLACE AS NEEDED. VERIFY W/OWNER
- 17 EXTERIOR LIGHTING TO BE PROVIDED BY MUSCO LIGHTING LUMINAIRE TYPE LED-24. VERIFY TYPE & LOCATION WITH OWNER & ELEC CONTRACTOR
- 18 HOSE BIB - SEE PLUMBING PLANS
- 19 10'-0" HIGH CMU WALL ENCLOSING WASH BAY. PROVIDE HORIZ. TIES @ 16" O.C. AND #5 BAR @ 24" O.C. VERT IN GROUTED CORES AND AT CORNERS. TOP COURSE TO HAVE BOND BEAM W/ (2) #5 BARS

REVISIONS

Rev #	Date	Description

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DAVENPORT, IA | 563.324.9028
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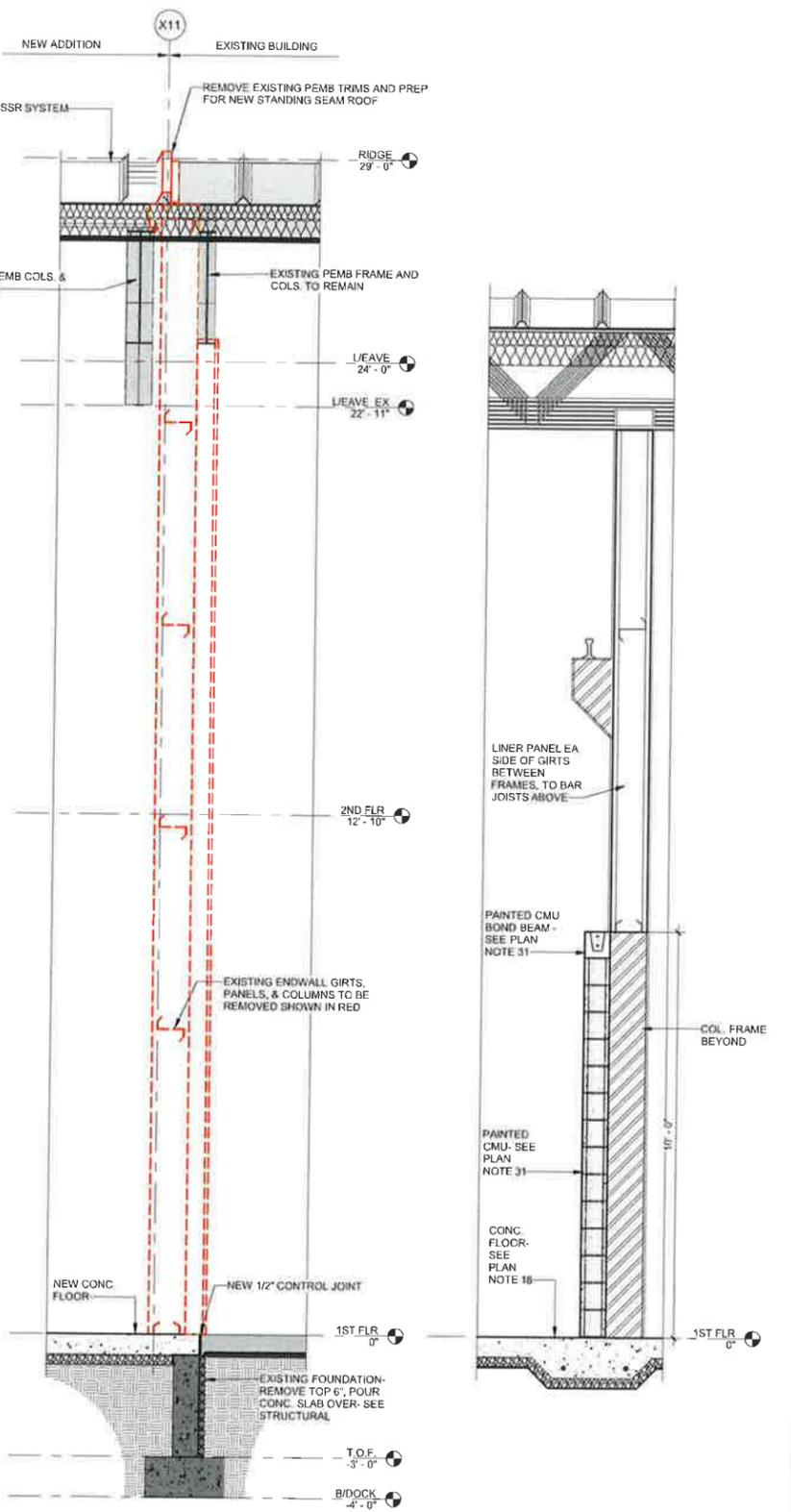
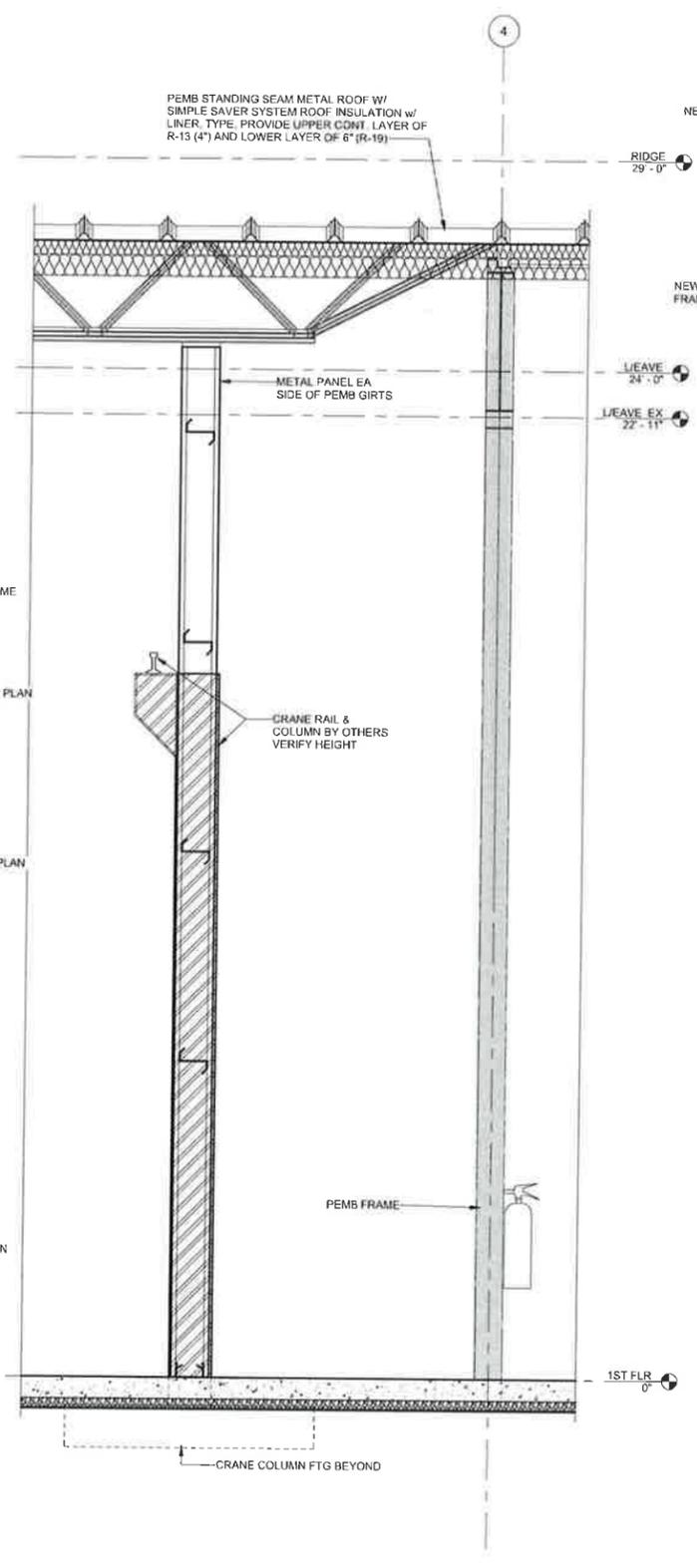
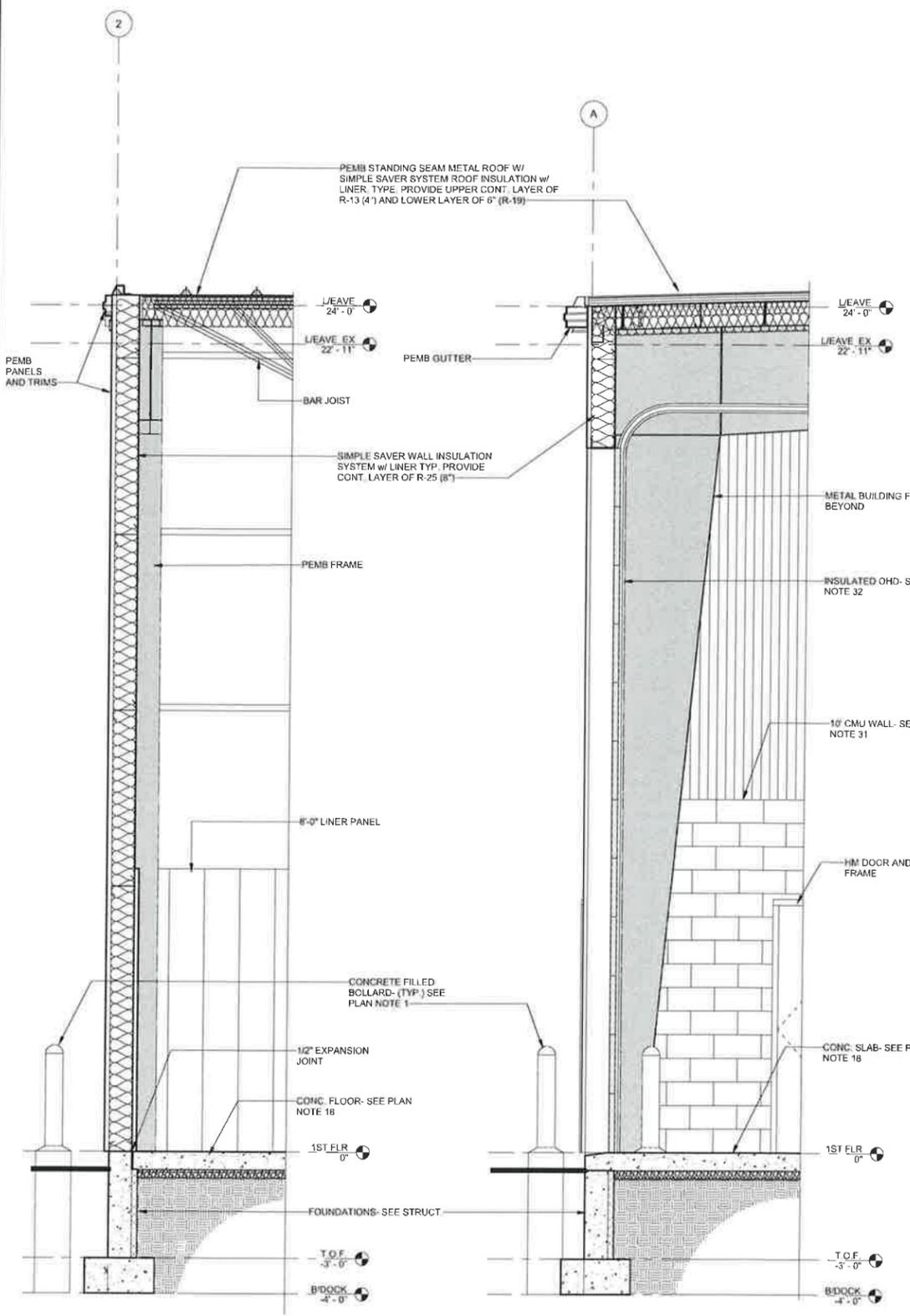
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BUILDING ELEVATIONS

SHEET
A3.0



1 WALL SECTION @ ENDWALL
A4.1 1/2" = 1'-0"

2 WALL SECTION @ SIDEWALL & OHD
A4.1 1/2" = 1'-0"

3 SECTION @ CRANE COLUMN
A4.1 1/2" = 1'-0"

4 SECTION @ EXISTING
A4.1 1/2" = 1'-0"

5 SECTION @ CMU WALL
A4.1 1/2" = 1'-0"

REVISIONS		
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ARCHITECTS
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2107 Stewart Rd
Muscatine, IA 52761

REVIEW SET

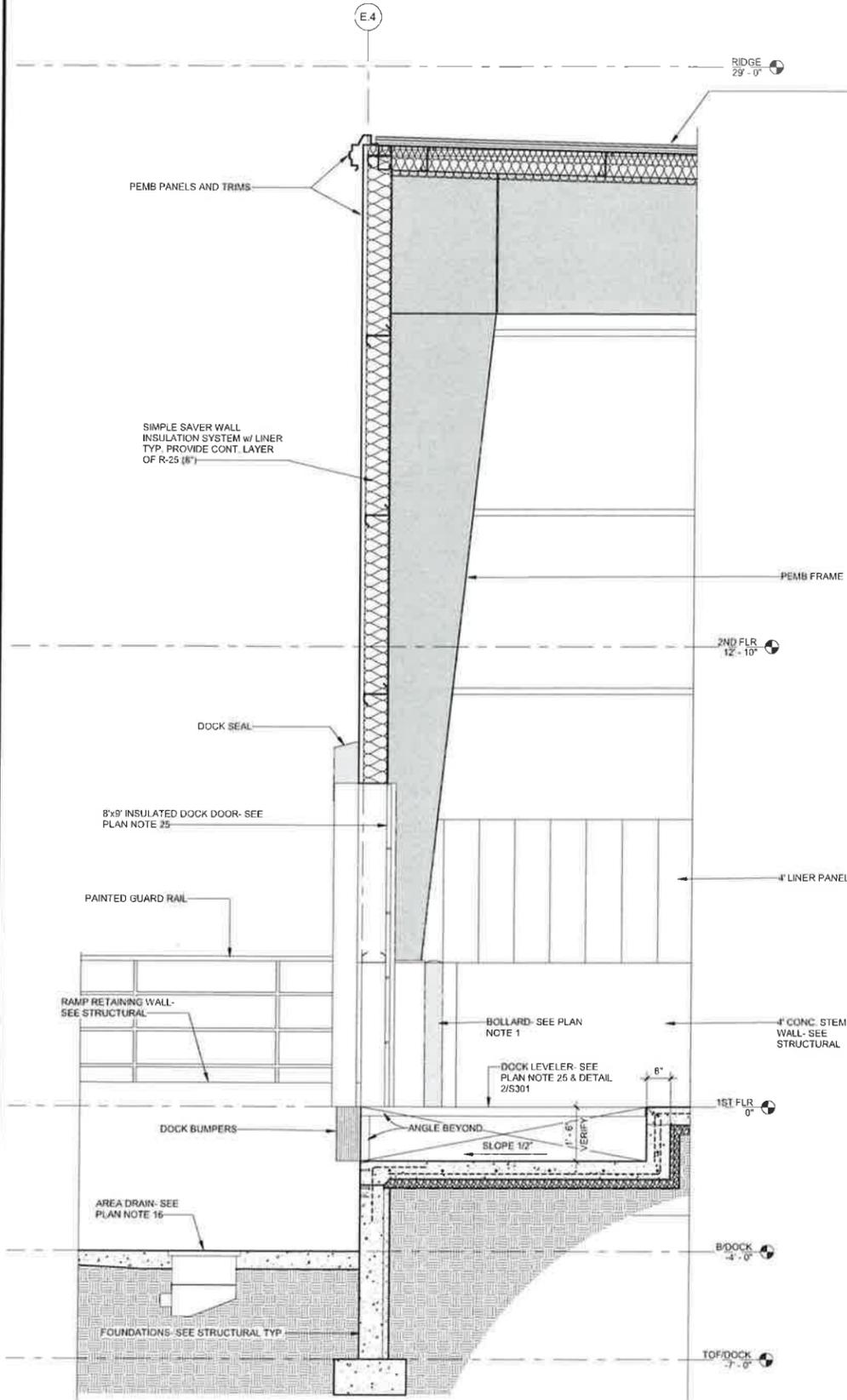
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DATE 09-21-19	JOB NO D.V.0012

WALL SECTIONS

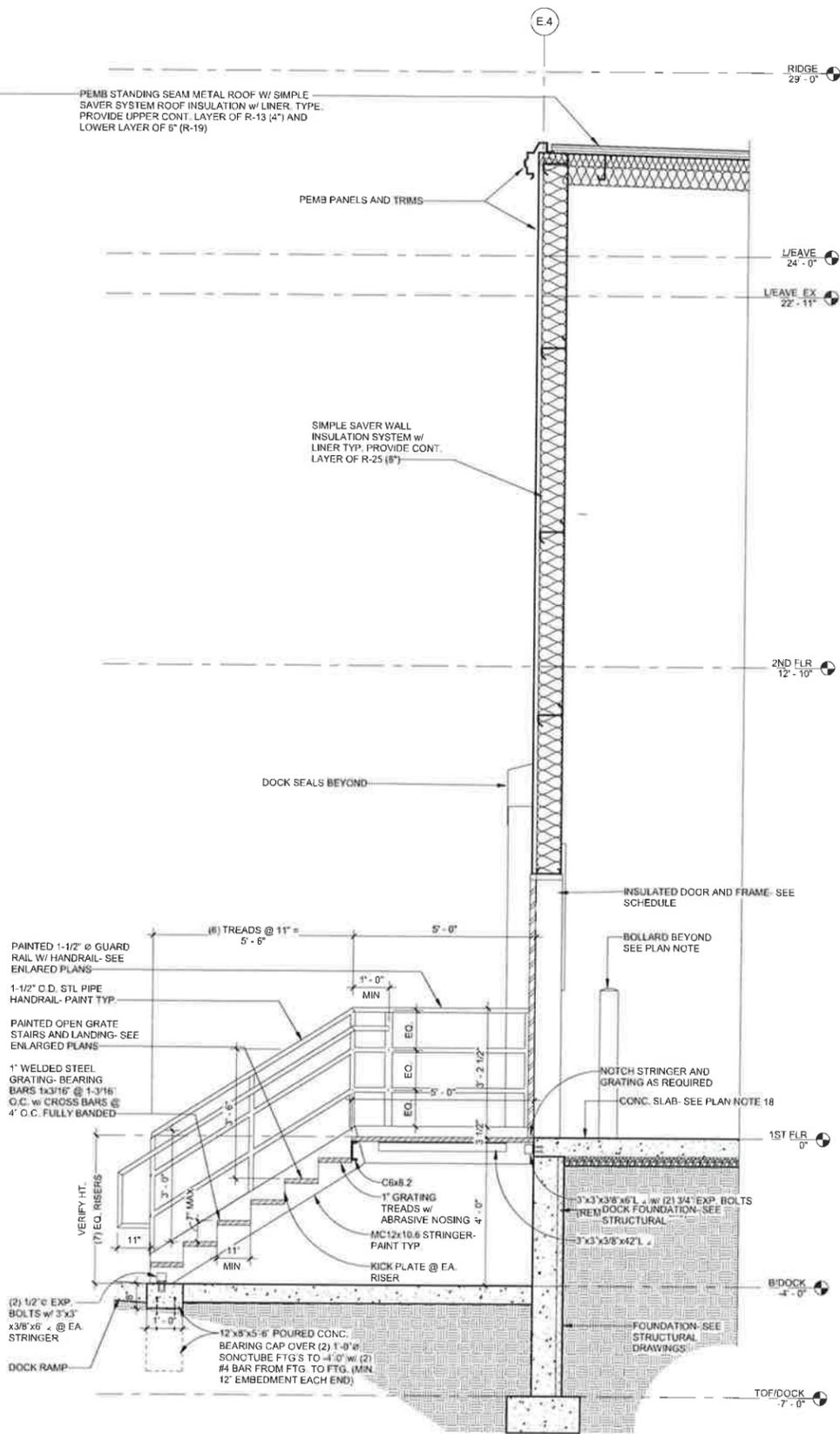
SHEET
A4.1

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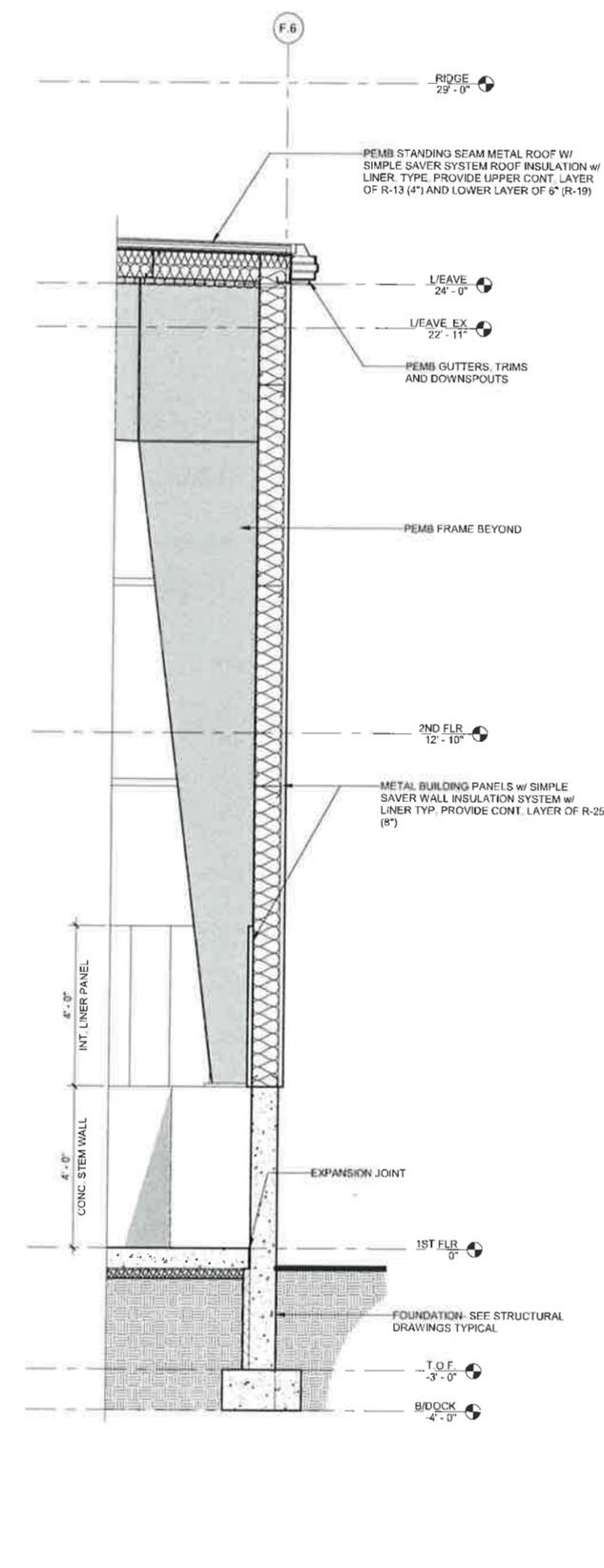
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1 WALL SECTION @ DOCK LEVEL
A4.2 1/2" = 1'-0"



2 WALL SECTION @ DOCK STAIR
A4.2 1/2" = 1'-0"



3 SECTION @ DOCK LOW EAVE
A4.2 1/2" = 1'-0"

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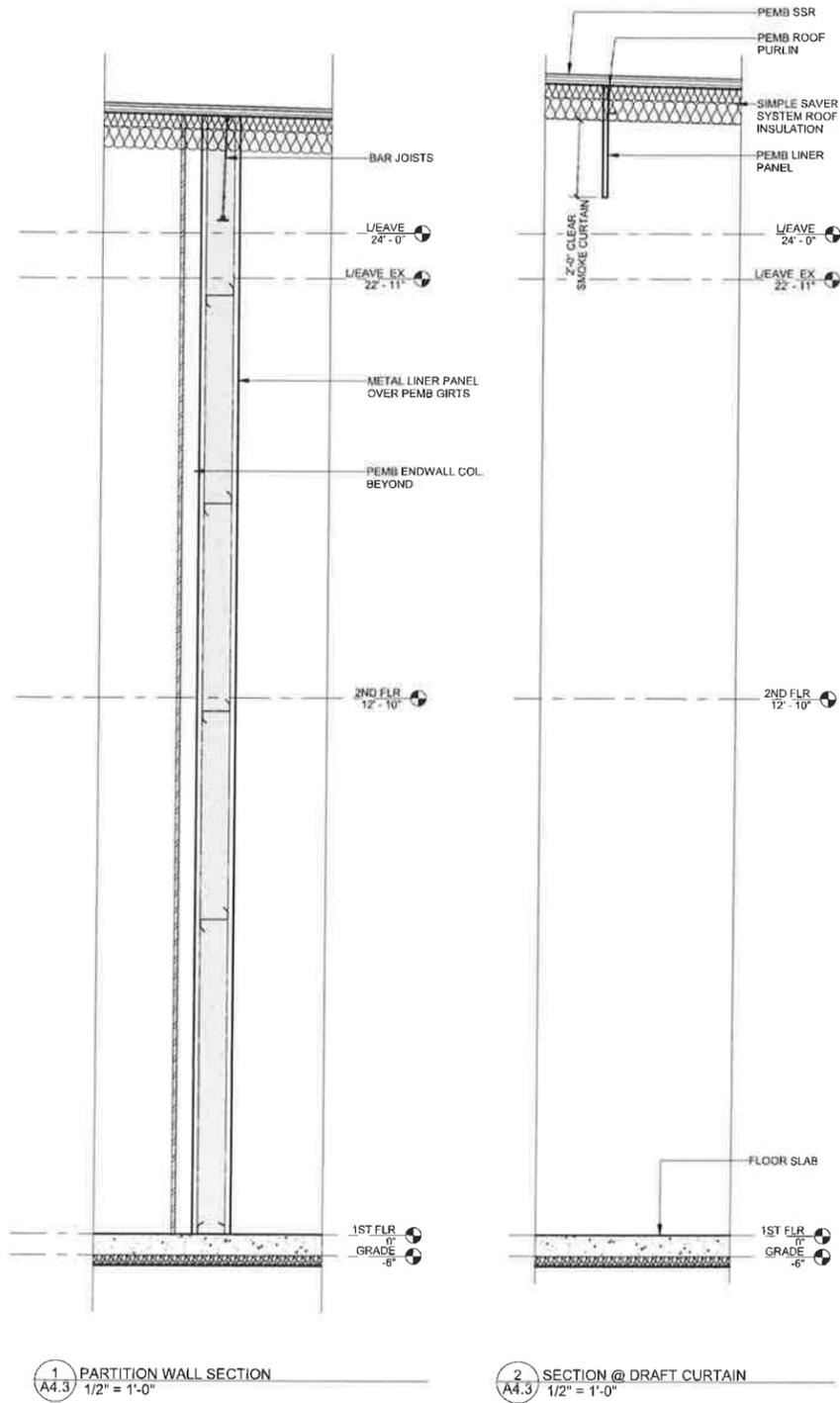
REVIEW SET

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WALL SECTIONS

SHEET
A4.2

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WALL SECTIONS

SHEET
A4.3

DOOR, FRAME & HARDWARE NOTES

HOLLOW METAL FRAMES NOTES AND SPECIFICATIONS:

- FIELD VERIFY EXISTING CONDITIONS PRIOR TO PERFORMING WORK. VERIFY THAT PRODUCTS SPECIFIED MATCH EXISTING PRODUCTS IN USE THAT ARE TO REMAIN AND/OR BE REUSED. NOTIFY ARCHITECT OF ANY DISCREPANCIES THAT ARISE.
- HOLLOW METAL FRAMES TO BE OF ONE PIECE WELDED CONSTRUCTION, GROUND SMOOTH, WITH MITERED CORNERS, 16 GA (INTERIOR AND EXTERIOR).
- PROVIDE ADEQUATE AND SUITABLE JAMB AND FLOOR ANCHORS. NOT LESS THAN THREE JAMB ANCHORS.
- FRAMES SHALL BE MORTISED, REINFORCED AND PREPARED TO RECEIVE HARDWARE. ENCLOSE STRIKE AND HINGE REINFORCEMENT WITH PLASTER GUARDS WHERE REQUIRED.
- PROVIDE RUBBER DOOR SILENCERS, MINIMUM OF THREE PER DOOR ON STRIKE JAMB, PROVIDE 2 ON HEADS OF DOUBLE-DOOR FRAMES.
- GROUT ALL FRAMES FULL WITH GROUT OR JOINT COMPOUND.
- PROVIDE LABELED FRAMES AS REQUIRED.
- PRIME PAINT: MANUFACTURER'S STANDARD, FACTORY APPLIED COAT COMPLYING WITH ANSI 250.10
- PRIME COAT TOUCH UP: IMMEDIATELY AFTER ERECTION, SAND SMOOTH RUSTED OR DAMAGED AREAS AND APPLY TOUCH-UP PRIMER.
- PROVIDE SUBMITTALS FOR REVIEW PRIOR TO FABRICATION, INCLUDING PRODUCT DATA AND SHOP DRAWINGS.

DOOR HARDWARE:

- FIELD VERIFY EXISTING CONDITIONS PRIOR TO PERFORMING WORK. VERIFY THAT PRODUCTS SPECIFIED MATCH EXISTING PRODUCTS IN USE THAT ARE TO REMAIN AND/OR BE REUSED. NOTIFY ARCHITECT OF ANY DISCREPANCIES THAT ARISE.
- FURNISH UL LISTED HARDWARE FOR ALL LABELED OPENINGS IN CONFORMANCE WITH THE REQUIREMENTS FOR THE CLASS OF OPENING SCHEDULED.
- ALL WORK SHALL BE IN ACCORDANCE WITH ALL APPLICABLE STATE AND LOCAL BUILDING CODES, INCLUDING ACCESSIBILITY CODES.
- COORDINATE REQUIRED REINFORCEMENT FOR DOORS AND FRAMES.
- COORDINATE KEYING WITH PROPERTY MANAGER AND TENANT.

STEEL DOORS:

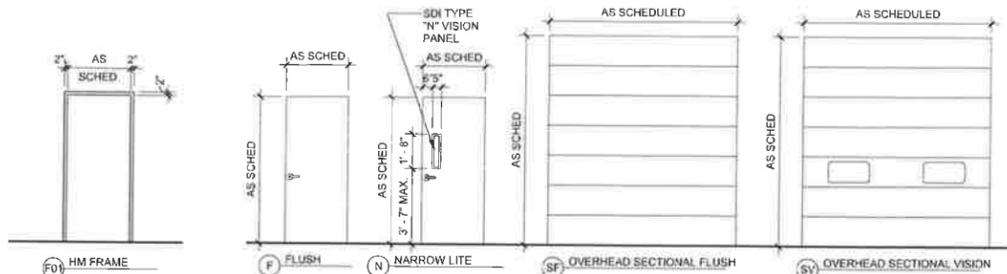
- PROVIDE 1 3/4 IN. (44 MM) THICK DOORS OF MATERIALS AND ANSIS/KE 100 GRADES AND MODELS SPECIFIED BELOW OR AS INDICATED ON DRAWINGS OR SCHEDULES.
- INTERIOR DOORS: GRADE II, HEAVY-DUTY, MODEL 2, SEAMLESS DESIGN, MIN 18 GA. COLD-ROLLED STEEL DOOR FACES
- EXTERIOR DOORS: GRADE III, EXTRA HEAVY-DUTY, MODEL 2, SEAMLESS DESIGN, MIN 14 GA. GALVANIZED STEEL SHEET FACES. CLOSE TOP EDGE OF EXTERIOR DOORS WITH INVERTED STEEL CHANNEL CLOSURE. SEAL JOINTS WEATHER TIGHT.

WOOD DOORS:

- FIELD VERIFY EXISTING CONDITIONS PRIOR TO PERFORMING WORK. VERIFY THAT PRODUCTS SPECIFIED MATCH EXISTING PRODUCTS IN USE THAT ARE TO REMAIN AND/OR BE REUSED. NOTIFY ARCHITECT OF ANY DISCREPANCIES THAT ARISE.
- PERFORM WORK IN COMPLIANCE WITH AWI: TYPE PC (PARTICLE CORE), TYPE PD (FIRE DOOR)
- FIRE RATED DOOR CONSTRUCTION LISTED AND LABELED BY TESTING AND INSPECTING AGENCY ACCEPTABLE TO AUTHORITIES HAVING JURISDICTION.
- WARRANTY: MANUFACTURER'S STANDARD FORM FOR DOOR DEFECTIVE IN MATERIALS OR WORKMANSHIP, EXCEEDING WARPING GREATER THAN 1/4" IN A 36" X 84" SECTION OR TELEGRAPHING OF CORE CONSTRUCTION IN FACE VENEER EXCEEDING .01 INCH IN A 3" SPAN.
- FACTORY MACHINE DOORS FOR HARDWARE. COORDINATE WITH DOOR HARDWARE SUPPLIER.
- PROVIDE SUBMITTALS FOR REVIEW PRIOR TO FABRICATION INCLUDING: PRODUCT DATA, SHOP DRAWINGS AND TWO 12" X 12" CORNER SECTION OF DOORS, WITH DOOR FACES AND EDGES REPRESENTING TYPICAL RANGE OF COLOR AND GRAIN, AND WITH FACTORY FINISH APPLIED, FOR EACH TYPE OF VENEER REQUIRED.
- FINISH: FACTORY FINISH TO COMPLY WITH REQUIREMENT OF AWI QUALITY STANDARDS - SECTION 1500 AND AWI TR-6 CATALYZED POLYURETHANE.
- PARTICLE CORE DOORS: 1-3/4" FLUSH WOOD INTERIOR DOOR WITH WOOD VENEER FACE PILES FOR TRANSPARENT FINISH, BONDED TO SOLID PARTICLE BOARD CORE COMPLYING WITH AWI SECTION 1300 TYPE PC-5 (HARDWOOD MATCHING EDGES) FOR NON-FIRE RATED DOORS. TYPE FD 1/3 FOR 20 MINUTE FIRE RATED DOORS WHERE INDICATED.

MINERAL CORE FOR:
 TYPE FD 3/4 FOR 45 MINUTE
 TYPE FD 1 FOR 60 MINUTE
 TYPE FD 1 1/2 FOR 90 MINUTE

AWI CUSTOM GRADE, VENEER GRADE A, FOR TRANSPARENT FINISH; TENANT STANDARD WOOD SPECIES, PLAIN SLICED, SLIP MATCHED



DOOR FRAMES 1/4" = 1'-0"
 DOOR TYPES 1/4" = 1'-0"

DOOR & FRAME SCHEDULE												
OPENING INFORMATION				PANEL INFORMATION				FRAME INFO				COMMENTS
NO.	ROOM NAME	WIDTH	HEIGHT	LEAFS	LEAF 1 TYPE 1	MATL	TYPE	MATL	HDWR	GLASS TYPE	FIRE RATING	
100A	WAREHOUSE	3'-0"	7'-0"	1	3'-0"	N	HM	F01	HM	X1		INSULATED DOOR AND FRAME
100B	WAREHOUSE	22'-0"	20'-0"	0	22'-0"	SV	STL	OH	STL			INSULATED OHD W/ OPENERS
100C	WAREHOUSE	22'-0"	20'-0"	0	22'-0"	SV	STL	OH	STL			INSULATED OHD W/ OPENERS
100D	WAREHOUSE	3'-0"	7'-0"	1	3'-0"	N	HM	F01	HM	AS		
100E	WAREHOUSE	22'-0"	20'-0"	0	22'-0"	SV	STL	OH	STL			INSULATED OHD W/ OPENERS
100F	WAREHOUSE	3'-0"	7'-0"	1	3'-0"	N	HM	F01	HM	X1		INSULATED DOOR AND FRAME
100G	WAREHOUSE	3'-0"	7'-0"	1	3'-0"	N	HM	F01	HM	X2		INSULATED DOOR AND FRAME
100H	WAREHOUSE	3'-0"	7'-0"	1	3'-0"	N	HM	F01	HM	X2		INSULATED DOOR AND FRAME
101A	DOCK	3'-0"	7'-0"	1	3'-0"	N	HM	F01	HM	X1		INSULATED DOOR AND FRAME
101B	DOCK	8'-0"	9'-0"									INSULATED DOOR W/ SEAL, LEVELER, RESTRAINT, LIGHTS AND OPENER
110	BREAK RM	3'-0"	7'-0"	1	3'-0"	F	HM	F01	HM	AS		
111A	CONF. 1	3'-0"	7'-0"	1	3'-0"	F	HM	F01	HM	A7		
111B	HALL	3'-0"	7'-0"	1	3'-0"	F	HM	F01	HM	A7		
112A	CONF. 2	3'-0"	7'-0"	1	3'-0"	F	HM	F01	HM	A7		
112B	CONF. 2	3'-0"	7'-0"	1	3'-0"	F	HM	F01	HM	A7		
113	MENS	3'-0"	7'-0"	1	3'-0"	F	HM	F01	HM	A7		
115	HALL	3'-0"	7'-0"	1	3'-0"	F	HM	F01	HM	A7		
EX-1	WAREHOUSE	16'-6"	16'-0"	0	16'-6"	SV	STL	OH	STL			EXISTING OHD TO REMAIN

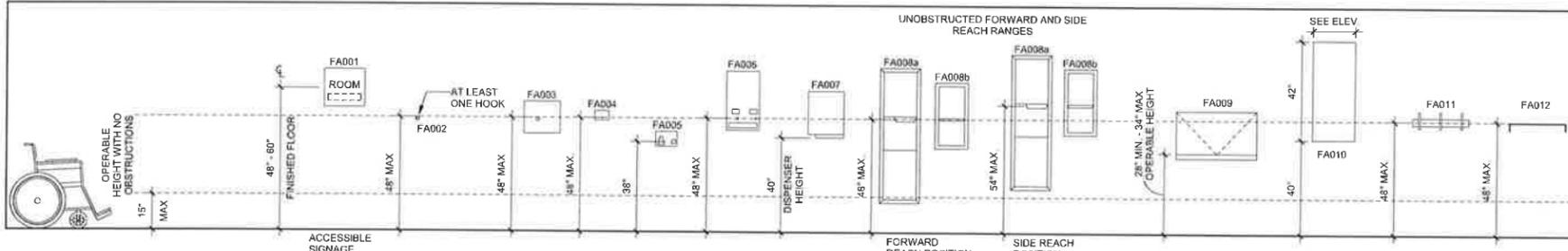
DOOR HARDWARE GROUPS

- X1 EXTERIOR - SECURE ACCESS
1 1/2 PAIR HINGES
LEVER SET - ENTRY FUNCTION
ELECTRIC STRIKE
EXIT DEVICE
TWO-WAY INTERCOM
CLOSER
THRESHOLD
WEATHERSTRIP
- X2 EXTERIOR - EGRESS ONLY
1 1/2 PAIR HINGES
EXIT DEVICE
NO EXTERIOR ACCESS
CLOSER
THRESHOLD
WEATHERSTRIP
- A1 INTERIOR - OFFICE FUNCTION
1 1/2 PAIR HINGES
LEVER SET - OFFICE FUNCTION
STOP
- A2 INTERIOR - OFFICE FUNCTION W/ CLOSER
1 1/2 PAIR HINGES
LEVER SET - OFFICE FUNCTION
CLOSER
STOP
- A3 INTERIOR - PRIVACY SET
1 1/2 PAIR HINGES
LEVER SET - PRIVACY FUNCTION
STOP
- A4 INTERIOR - PRIVACY SET W/ CLOSER
1 1/2 PAIR HINGES
LEVER SET - PASSAGE FUNCTION
CLOSER
STOP
- A5 INTERIOR - PASSAGE SET
1 1/2 PAIR HINGES
LEVER SET - PASSAGE FUNCTION
STOP
- A6 INTERIOR - PASSAGE SET W/ CLOSER
1 1/2 PAIR HINGES
LEVER SET - PASSAGE FUNCTION
CLOSER
STOP
- A7 INTERIOR - PUSH/PULL
1 1/2 PAIR HINGES
PUSH PLATE / PULL LEVER
KICKPLATE
CLOSE
STOP
- A8 INTERIOR - STORAGE
1 1/2 PAIR HINGES
LEVER SET - STORAGE FUNCTION
CLOSER
- A9 INTERIOR - STORAGE (PAIR)
3 PAIR HINGES
ACTIVE LEAF
LEVER SET - STORAGE FUNCTION
INACTIVE LEAF
1 SET HEADTOE BOLTS
1 FLOOR OR WALL STOP (EACH LEAF - WHERE APPROPRIATE)

FIXTURE ACCESSORIES LEGEND

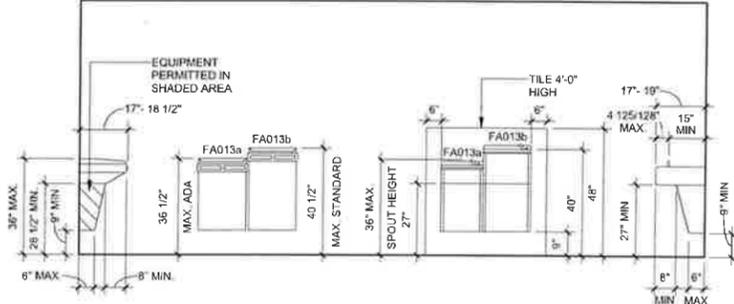
- | SYMBOL | DESCRIPTION |
|--------|--|
| FA001 | ACCESSIBLE SIGNAGE |
| FA002 | ACCESSIBLE CLOTHING RACK HOOK ROD AND SHELF |
| FA003 | ACCESSIBLE EYE WASH FIRST AID / MISC |
| FA004 | SOAP DISP |
| FA005 | ELECTRIC HAND DRYER |
| FA006 | SANITARY NAPKIN DISPENSER |
| FA007 | ACCESSIBLE PAPER TOWEL DISPENSER |
| FA008a | COMBINATION PAPER TOWEL DISPENSER / WASTE RECEPTACLE |
| FA008b | PAPER TOWEL DISPENSER |

FIXTURE ACCESSORIES

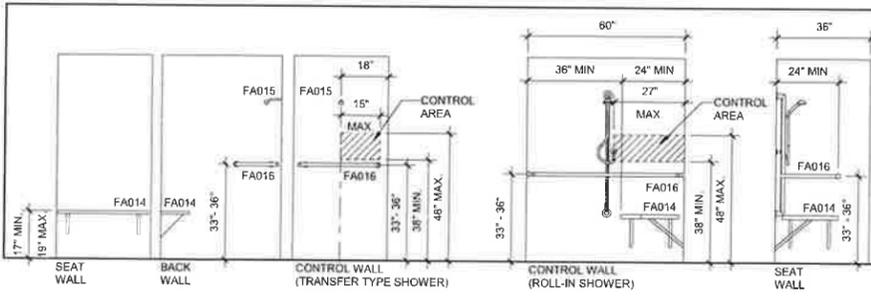


- | SYMBOL | DESCRIPTION |
|--------|--------------------------|
| FA009 | ACCESSIBLE BABY CHANGING |
| FA010 | MIRROR |
| FA011 | HAT & COAT RACK |
| FA012 | SHELF |
| FA013a | ACCESSIBLE WATER COOLER |
| FA013b | WATER COOLER |
| FA014 | ADA SHOWER SEAT |
| FA015 | SHOWER HEAD |
| FA016 | GRAB BAR |

WATER COOLERS

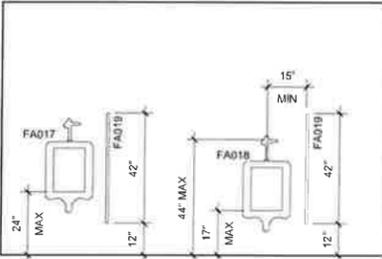


SHOWER

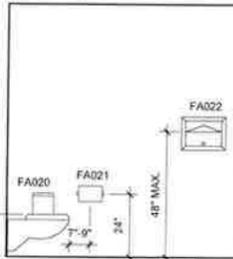


- | SYMBOL | DESCRIPTION |
|--------|---|
| FA017 | STANDARD URINAL |
| FA018 | ACCESSIBLE URINAL |
| FA019 | URINAL SCREEN |
| FA020 | SANITARY NAPKIN DISPOSAL (ONLY IN WOMENS) |
| FA021 | TOILET PAPER DISPENSER |
| FA022 | TOILET SEAT COVER DISPENSER |
| FA023 | 24"x36" MIRROR |

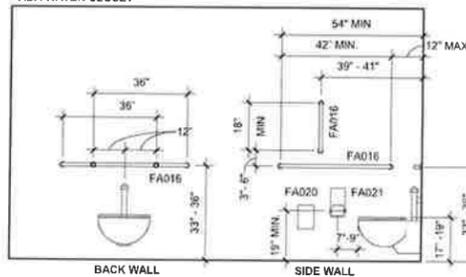
URINALS & SCREENS



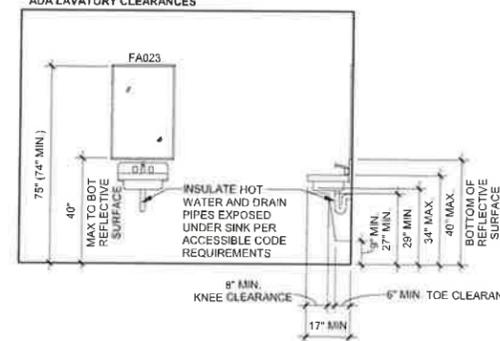
STANDARD WATER CLOSET



ADA WATER CLOSET



ADA LAVATORY CLEARANCES



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DOOR SCHEDULE and ADA MNTG HEIGHTS

SHEET
A8.0

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BUILDING ADDITION MUSCO LIGHTING MUSCATINE, IOWA

PROJECT ENGINEER

MICHAEL W. JANECEK, P.E.
MARTIN & WHITACRE SURVEYORS & ENGINEERS, INC
563-263-7691

CITY OF MUSCATINE CONTACTS

JIM EDGMOND, CITY ENGINEER
PUBLIC WORKS 563-263-8933

MUSCATINE POWER & WATER:

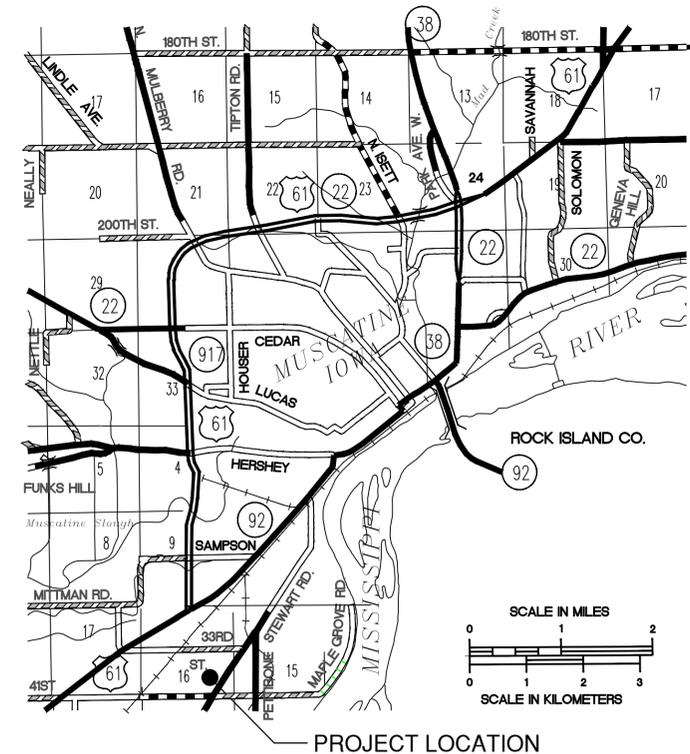
WATER: BRYAN BUTLER 563-262-3360
ELECTRIC: STEVE SHOPPA 563-262-3324
COMMUNICATIONS: TOM LEWIS 563-262-3206

ALLIANT ENERGY

GAS JOHN CARNEY 563-288-3322

CENTURY LINK

COMMUNICATIONS 563-355-1893



VICINITY MAP
NO SCALE

PROJECT DESCRIPTION: ADDITION OF NEW BUILDING
LOCATION: 2107 STEWART ROAD



PROJECT LOCATION



THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH CURRENT IOWA STATEWIDE URBAN DESIGN STANDARDS AND SPECIFICATIONS, CITY BIDDING DOCUMENTS AND PROJECT SPECIAL CONDITIONS

THIS PROJECT SHALL OPERATE UNDER NPDES GENERAL PERMIT NO. 2 FOR "STORMWATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FOR CONSTRUCTION ACTIVITIES" THE DEVELOPER HAS FILED A PERMIT, ALL CONTRACTORS SHALL SIGN AND ADHERE TO PROJECT SWPPP

CONTRACTORS SHALL CALL IOWA ONE CALL 1-800-292-8989 PRIOR TO START OF WORK. EXISTING UTILITIES SHOWN ARE DERIVED FROM SURVEY & MAPPING DATA. FIELD LOCATE AND VERIFY ALL UTILITIES SHOWN. THE CONTRACTOR SHALL PROTECT EXISTING UTILITIES AND REPAIR DAMAGES.

CONTRACTORS SHALL PROVIDE TEMPORARY FENCING TO PREVENT ENTRANCE BY PEDESTRIANS AND VEHICLES. ALL WORK SHALL MEET CITY SAFETY PROTOCOLS.

BID LETTING INFORMATION

BID LETTING DATE/TIME: TBD

BIDDER QUESTIONS SHOULD BE DIRECTED TO MARTIN & WHITACRE SURVEYORS & ENGINEERS, INC

BID SHALL BE HAND DELIVERED OR MAILED TO VANTAGE ARCHITECTS, LLC

SHEET INDEX

SHEET NO.	SHEET TITLE
A.01	TITLE SHEET
A.02	LEGEND AND GENERAL NOTES
C.01	SITE PLAN
F.01	DEMOLITION PLAN
L.01	PAVING PLAN
M.01	STORM SEWER & DETENTION PLAN
Q.01	EROSION CONTROL PLAN
U.01	DETAILS

I HEREBY CERTIFY THAT THIS ENGINEERING DOCUMENT WAS PREPARED BY ME OR UNDER MY DIRECT PERSONAL SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF IOWA

MICHAEL W JANECEK, P.E. IA. REG. NO 15289 DATE

MY LICENSE RENEWAL DATE IS DECEMBER 31, 2019

Pages or sheets covered by this seal: _____

A.01 THROUGH U.01

BUILDING ADDITION
MUSCO LIGHTING
MUSCATINE, IOWA
8401.19

Martin & Whitacre
Surveyors & Engineers, Inc.
MUSCATINE, IOWA
P.O. BOX 413
VOICE (563) 263-7691
EMAIL info@martin-whitacre.com



REVISIONS:

- 1
- 2
- 3
- 4

DATE: 7/22/2019

JOB NO: 8401.19

SHEETS: 8

SHEET: A.01

EXISTING TOPOGRAPHIC/UTILITIES

	EXISTING PROPERTY LINE
	EXISTING EASEMENT LINE
	EXISTING SETBACK LINE
	EXISTING FENCE LINE
	EXISTING TELEPHONE LINE
	EXISTING FIBER OPTIC LINE
	EXISTING UNDERGROUND ELECTRIC
	EXISTING OVERHEAD ELECTRIC
	EXISTING UNDERGROUND GAS LINE
	EXISTING WATER MAIN OR SERVICE
	EXISTING STORM SEWER/CULVERT
	EXISTING SANITARY SEWER
	EXISTING TELEPHONE RISER
	EXISTING CABLE TV RISER
	EXISTING ELECTRIC METER
	EXISTING ELECTRIC TRANSFORMER
	EXISTING ELECTRICAL MANHOLE
	EXISTING POWER POLE
	EXISTING LIGHT POLE
	EXISTING GAS METER
	EXISTING WATER VALVE
	EXISTING CURB BOX
	EXISTING FIRE HYDRANT
	EXISTING STORM SEWER INTAKE
	EXISTING STORM SEWER MANHOLE
	EXISTING SANITARY MANHOLE
	SIGN

CONTOUR LINES

	EXISTING MAJOR CONTOUR LINE
	EXISTING MINOR CONTOUR LINE
	PROPOSED MAJOR CONTOUR LINE
	PROPOSED MINOR CONTOUR LINE

COMMON ABBREVIATIONS

PCC	PORTLAND CONCRETE CEMENT
HMA	HOT MIX ASPHALT
DIP	DUCTILE IRON PIPE
RCP	REINFORCED CONCRETE PIPE
PVC	POLY VINYL CHLORIDE PIPE
CMP	CORRUGATED METAL PIPE
PE	POLYETHYLENE PIPE
HDPE	HIGH DENSITY POLYETHYLENE PIPE
IDOT	IOWA DEPARTMENT OF TRANSPORTATION
SUDAS	STATEWIDE URBAN DESIGN AND SPECIFICATIONS
LUST	LEAKING UNDERGROUND STORAGE TANK
MPW	MUSCATINE POWER & WATER

PROPOSED TOPOGRAPHIC/UTILITIES

	PROPOSED FENCE LINE
	PROPOSED TELEPHONE LINE
	PROPOSED FIBER OPTIC LINE
	PROPOSED UNDERGROUND ELECTRIC
	PROPOSED OVERHEAD ELECTRIC
	PROPOSED UNDERGROUND GAS LINE
	PROPOSED WATER MAIN OR SERVICE
	PROPOSED STORM SEWER/CULVERT
	PROPOSED SANITARY SEWER
	PROPOSED TELEPHONE RISER
	PROPOSED CABLE TV RISER
	PROPOSED COMMUNICATION STORAGE VAULT
	PROPOSED COMMUNICATION HANDHOLE
	PROPOSED COMMUNICATION FLOWER POT
	PROPOSED ELECTRIC METER
	PROPOSED ELECTRIC TRANSFORMER
	PROPOSED ELECTRIC HANDHOLE
	PROPOSED ELECTRICAL MANHOLE
	PROPOSED POWER POLE
	PROPOSED LIGHT POLE
	PROPOSED GAS METER
	PROPOSED WATER VALVE
	PROPOSED CURB BOX
	PROPOSED FIRE HYDRANT
	PROPOSED SW-501 STORM INTAKE
	PROPOSED SW-505 DOUBLE STORM INTAKE
	PROPOSED SW-507 STORM INTAKE
	PROPOSED SW-510 STORM INTAKE
	PROPOSED FLARED END
	PROPOSED STORM SEWER MANHOLE
	PROPOSED SANITARY MANHOLE
	SIGN

HATCHES

	PROPOSED PCC PAVEMENT HATCH
	PROPOSED HMA PAVEMENT HATCH
	PROPOSED CURB & GUTTER HATCH
	PAVEMENT REMOVAL
	GRAVEL REMOVAL
	UTILITY REMOVAL

LANDSCAPING

	PROPOSED DECIDUOUS TREE
	PROPOSED SHRUB

GENERAL NOTES AND PROJECT SPECIFICATIONS

- THE INTENT OF THE PLANS AND SPECIFICATIONS IS TO PROVIDE CONSTRUCTION DOCUMENT FOR A GENERAL CONTRACTOR UNDER CONTRACT WITH NPSW LLC.
- ALL WORK SHALL FOLLOW CURRENT POSTED IOWA STATEWIDE URBAN DESIGN AND SPECIFICATIONS (SUDAS) FOR MATERIALS AND EXECUTION.
- CONSTRUCTION STAKING WILL BE PROVIDED BY A LICENSED LAND SURVEYOR AT THE OWNER'S EXPENSE.
- THE PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF MUSCATINE REQUIREMENTS, THE IOWA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION" LATEST ADDITION; IOWA SUDAS SPECIFICATIONS; THE CONTRACT DOCUMENTS AND THESE DRAWINGS.
- THE CONTRACTOR SHALL NOTIFY THE CITY ENGINEER FOR THE CITY OF MUSCATINE AND ALL UTILITY COMPANIES AT LEAST 48 HOURS PRIOR TO THE START OF CONSTRUCTION.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT ALL EXISTING UTILITIES AND PAVED STREETS, INCLUDING ANY NOT SHOWN ON THESE DRAWINGS. THE CONTRACTOR SHALL VERIFY ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION AND NOTIFY THE ENGINEER IF ANY CONFLICTS WITH THE DRAWINGS OCCUR. ANY DAMAGE TO EXISTING UTILITIES CAUSED BY TRENCHING AND GRADING OPERATIONS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. EXISTING UTILITY LOCATIONS SHOWN ON THE DRAWINGS ARE APPROXIMATE. IT IS POSSIBLE THAT THERE ARE OTHERS UTILITIES WHOSE EXISTENCE IS PRESENTLY NOT KNOWN OR SHOWN. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE TYPE AND LOCATION OF UNDERGROUND UTILITIES AND TO AVOID DAMAGE THERETO. MINOR CONFLICT WITH UNDERGROUND UTILITIES THAT ARE ABANDONED BUT NOT REMOVED BY THE UTILITY COMPANY SHALL BE REMOVED AS NECESSARY FOR THE CONSTRUCTION OF THE PROJECT BY THE CONTRACTOR. REMOVAL SHALL BE INCIDENTAL TO THE CONTRACT AND SHALL NOT ENTITLE THE CONTRACTOR TO ANY EXTRA COMPENSATION.
- THE CITY AND UTILITY COMPANIES SHALL HAVE ACCESS TO THE PROJECT FOR CONSTRUCTION AND TO MAKE EXTENSIONS OR ADJUSTMENTS OF SERVICES. NO CLAIM FOR ADDITIONAL COMPENSATION WILL BE ALLOWED TO THE CONTRACTOR FOR ANY INTERFERENCE OR DELAY CAUSED BY SUCH WORK.
- ALL UNSUITABLE MATERIAL, CONSTRUCTION DEBRIS RESULTING FROM CONSTRUCTION OPERATIONS, AND EXISTING PAVEMENT, SIDEWALKS AND DRIVEWAY PAVEMENT SCHEDULED FOR REMOVAL SHALL BE PROPERLY DISPOSED OF OFF-SITE IN ACCORDANCE WITH LOCAL, STATE AND FEDERAL LAWS AND GUIDELINES. THE CONTRACTOR SHALL DISPOSE OF REMOVED CONCRETE IN A METHOD APPROVED BY THE IOWA DEPARTMENT OF NATURAL RESOURCES.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE WASTE AREAS OR DISPOSAL SITES FOR EXCESS OR UNDESIRABLE MATERIAL, WHICH IS NOT TO BE INCORPORATED IN THE WORK INVOLVED IN THIS PROJECT. NO PAYMENT FOR OVERHAUL WILL BE ALLOWED FOR MATERIAL HAULED TO THESE SITES.
- PROVIDE POSITIVE DRAINAGE AT ALL TIMES WITHIN THE CONSTRUCTIONS AREAS. DO NOT ALLOW WATER TO DRAIN OR TO POND ONTO PRIVATE PROPERTY.
- EROSION CONTROL FEATURES SHALL BE INCORPORATED INTO THE CONSTRUCTION AT THE EARLIEST POSSIBLE TIME TO LIMIT EROSION AND STREAM POLLUTION AND SHALL BE MAINTAINED UNTIL ACCEPTANCE OF THE PROJECT BY THE CITY.
- ALL STORM SEWER INLETS THAT RECEIVE DRAINAGE FROM EXPOSED EARTH AREAS SHALL BE PROTECTED FROM INTAKE OF ERODED SOIL WITH COMPOST FILTER SOCK OR OTHER ACCEPTABLE INLET PROTECTION MEASURES. THE INLET PROTECTION SHALL BE MAINTAINED ACCORDING TO THE PROJECT SWPPP.
- WHEN NOT INITIATED AT AN EXISTING JOINT, REMOVAL OF EXISTING CONCRETE PAVEMENT, ASPHALT OR CONCRETE DRIVES AND SIDEWALKS SHALL BE INITIATED WITH FULL DEPTH SAW CUTS. EXERCISING EXTREME CARE TO AVOID SURFACE BREAKS AND SPALLING. IF SURFACE BREAKS OR SPALLING OCCUR, ADDITIONAL REMOVAL AND REPLACEMENT SHALL BE MADE AT THE CONTRACTOR'S EXPENSE. THE COST OF SAW CUTS SHALL BE CONSIDERED INCIDENTAL TO THE COST OF CONSTRUCTION.
- THE CONTRACTOR SHALL PREVENT SILT AND DEBRIS FROM ENTERING STORM AND SANITARY SEWERS AND SHALL REMOVE ANY SUCH MATERIAL THAT ENTERS THE SEWERS. NO ADDITIONAL PAYMENT WILL BE MADE FOR THIS WORK.
- CARE SHALL BE TAKEN TO PREVENT STORM WATER FROM UNDERMINING THE PAVEMENT DURING CONSTRUCTION. IF AREAS BECOME UNDERMINED, THEY SHALL BE REPAIRED IN A MANNER ACCEPTABLE TO THE ENGINEER. PRESSURE GROUTING MY BE REQUIRED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ADJACENT STREETS AND PROPERTY FREE FROM EXCESSIVE BUILD-UP OF SOIL AND/OR GRANULAR MATERIAL FROM THE PROJECT. THE CONTRACTOR SHALL CLEAN EXCESS SOIL AND OTHER MATERIAL FROM THE ADJACENT STREETS ON A DAILY BASIS TO THE SATISFACTION OF THE ENGINEER. ANY DAMAGE DONE TO EXISTING STREETS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL TAKES MEASURES TO PREVENT THE SPREAD OF DUST FROM THE CONSTRUCTION AREAS AND HAUL ROUTES, WHICH MAY INCLUDE THE APPLICATION OF WATER AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- SPECIAL CARE SHALL BE TAKEN WHEN FORMING AT INTERSECTIONS SO THAT THE PROFILES AND GRADES OBTAINED ARE AS SHOWN ON THE PLANS AND GRADE SHEETS. SHORT LENGTH OF FORMS OR FLEXIBLE FORMS MAY BE NECESSARY AT THESE LOCATIONS.
- ANY PROPERTY DAMAGE OR DISTURBANCE TO PRIVATE PROPERTY OUTSIDE OF GRADING/CONSTRUCTION LIMITS SHALL REQUIRE THE CONTRACTOR TO RESTORE THE PROPERTY TO ORIGINAL OR BETTER CONDITION AT HIS OWN EXPENSE.
- THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY IF ANY DISCREPANCIES ARE ENCOUNTERED BETWEEN THE PLANS AND SPECIFICATIONS AND THE EXISTING FIELD CONDITIONS.
- CONSTRUCTION STAKING, INSPECTION AND TESTING SHALL BE THE RESPONSIBILITY OF THE OWNER.
- TRAFFIC CONTROL SHALL BE PROVIDED IN ACCORDANCE WITH THE DETAILS AND STANDARDS SHOWN IN THESE PLANS AND THE CONTRACT DOCUMENTS, AND IN COMPLIANCE WITH IOWA DEPARTMENT OF TRANSPORTATION AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.).
- PROPERTY CORNERS SHALL BE PROTECTED DURING CONSTRUCTION. PROPERTY CORNERS DISTURBED BY CONTRACTOR OPERATIONS SHALL BE REPLACED AT HIS OWN EXPENSE.
- ALL DIMENSIONS AND RADIUS MEASUREMENTS ARE TO THE BACK OF CURB UNLESS OTHERWISE SHOWN.
- RIP RAP SHALL BE INSTALLED PER PLANS.
- MANUFACTURED SAND BACKFILL IS REQUIRED FOR ALL TRENCH BACKFILL WITHIN 10-FEET OF BACK OF CURB ON ANY CITY STREET.
- ALL STORM SEWER SHALL BE A2000 PVC, RCP, OR APPROVED EQUAL. VERIFY SIZE AND MATERIAL TYPE WITH MECHANICAL PLANS AND LOCAL PLUMBING CODE.
- ALL WATER MAIN SHALL BE SDR 21 PVC OR APPROVED EQUAL. VERIFY SIZE AND MATERIAL TYPE WITH MECHANICAL PLANS AND LOCAL PLUMBING CODE.
- ALL SANITARY SEWER LATERALS SHALL BE SDR 23.5 PVC OR APPROVED EQUAL. VERIFY SIZE AND MATERIAL TYPE WITH MECHANICAL PLANS AND LOCAL PLUMBING CODE.

F:\CVI\3D PROJECTS\8401 - MUSCO LIGHTING\3 PLAN SHEETS\PLAN SET.DWG PLOTTED: 7/22/2019

#	DATE	DESCRIPTION	BY
		DESIGNED BY:	JMS
		DRAWN BY:	JMS
		PROJECT ENGR:	MWJ

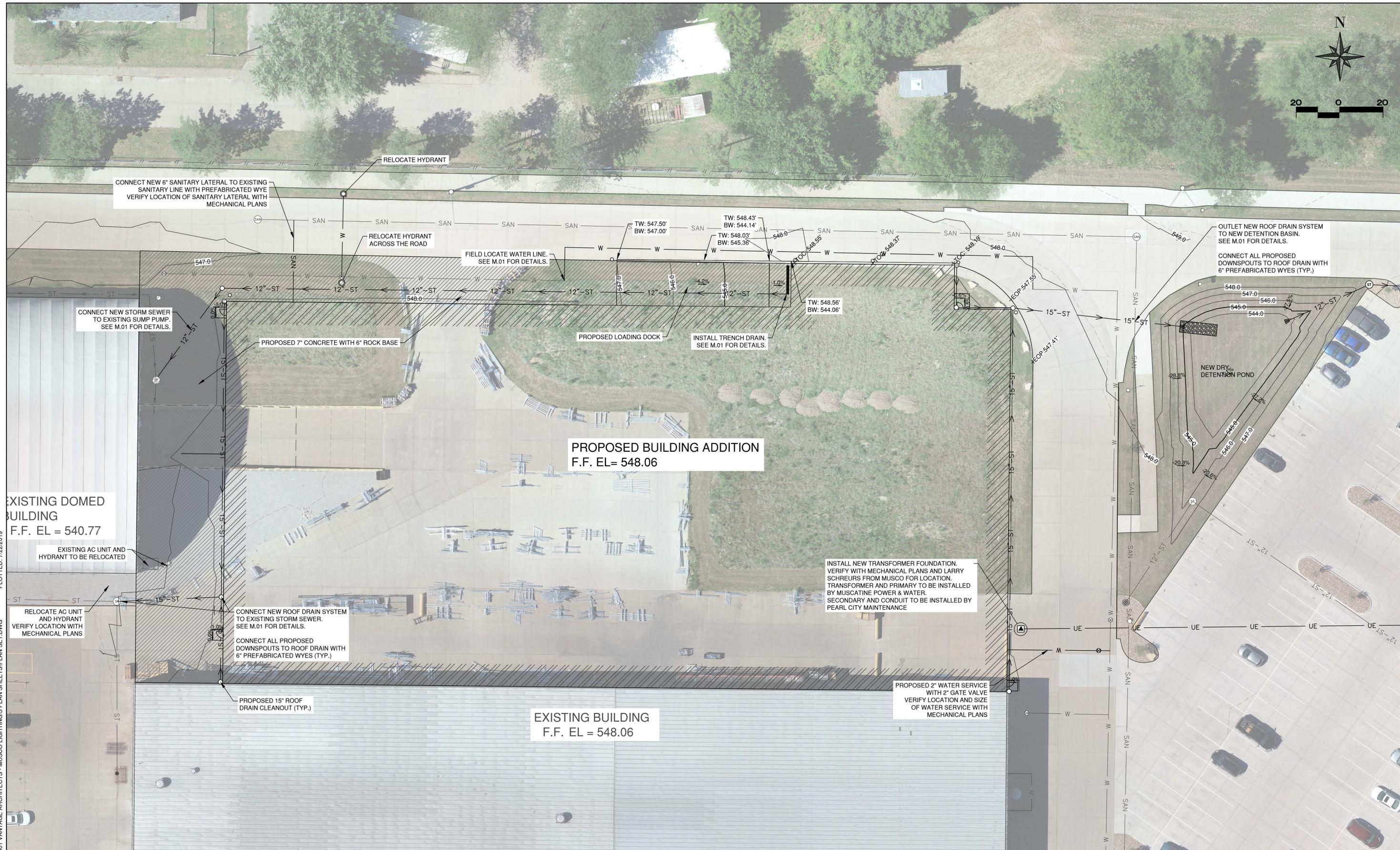
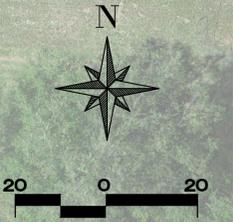


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BUILDING ADDITION
MUSCO LIGHTING
MUSCATINE, IOWA

LEGEND AND GENERAL NOTES

PROJ NO:	8401.19	DRAWING NO:	A.02
CONST PROJ:	000		
SCALE:	AS NOTED	SHEET NO:	2 OF 8
DATE:	7/22/2019		



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#	DATE	DESCRIPTION	BY

DESIGNED BY: JMS
 DRAWN BY: JMS
 PROJECT ENGR: MWJ

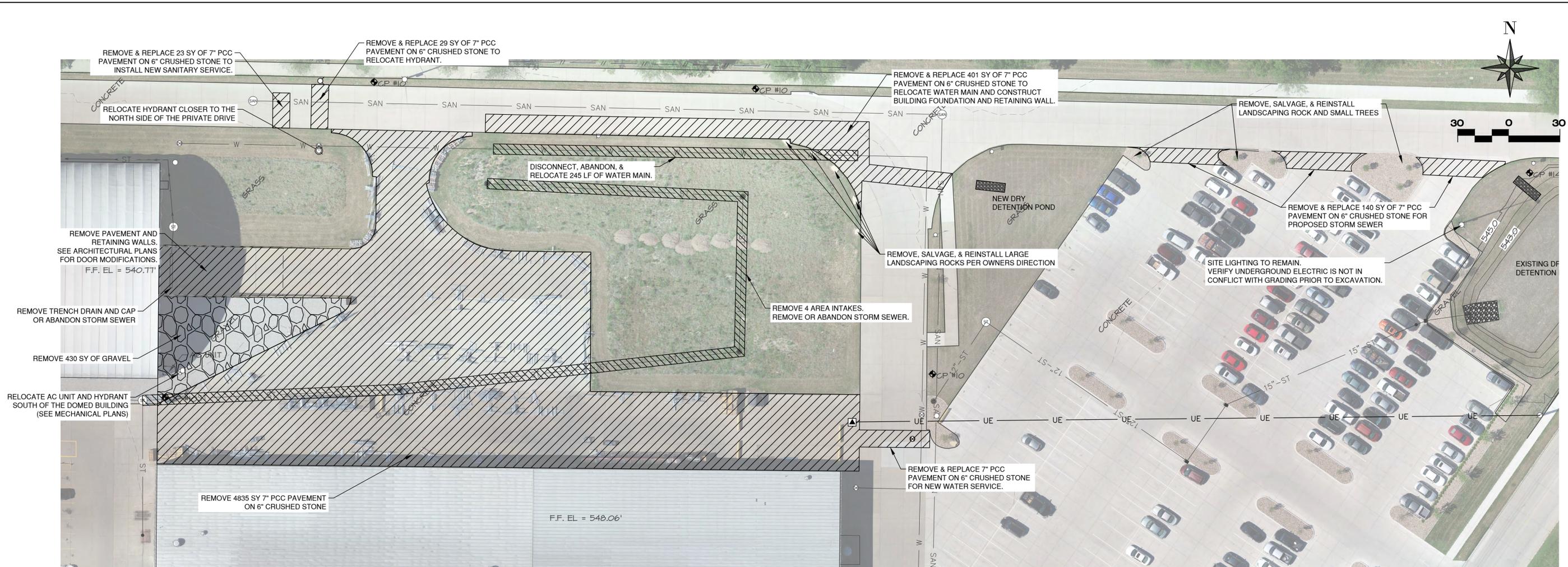


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BUILDING ADDITION
 MUSCO LIGHTING
 MUSCATINE, IOWA

SITE PLAN

PROJ NO:	8401.19	DRAWING NO:	C.01
CONST PROJ:	000		
SCALE:	AS NOTED	SHEET NO:	3 OF 8
DATE:	7/19/2019		



GENERAL DEMOLITION NOTES:

1. CLEAR AND GRUB ALL AREAS WITHIN THE CONSTRUCTION LIMITS
2. OWNER TO RELOCATE LIGHTING EQUIPMENT, DUMPSTER, & BARRICADES STORED WITHIN PROJECT LIMITS.
3. VERIFY PROPOSED SEWER & WATER SERVICE LOCATIONS PRIOR TO DEMOLITION OF ROADWAYS.

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#	DATE	DESCRIPTION	BY	DESIGNED BY:
				JMS
				DRAWN BY:
				JMS
				PROJECT ENGR:
				MWJ

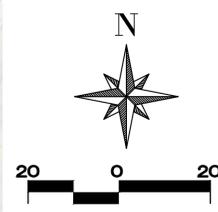
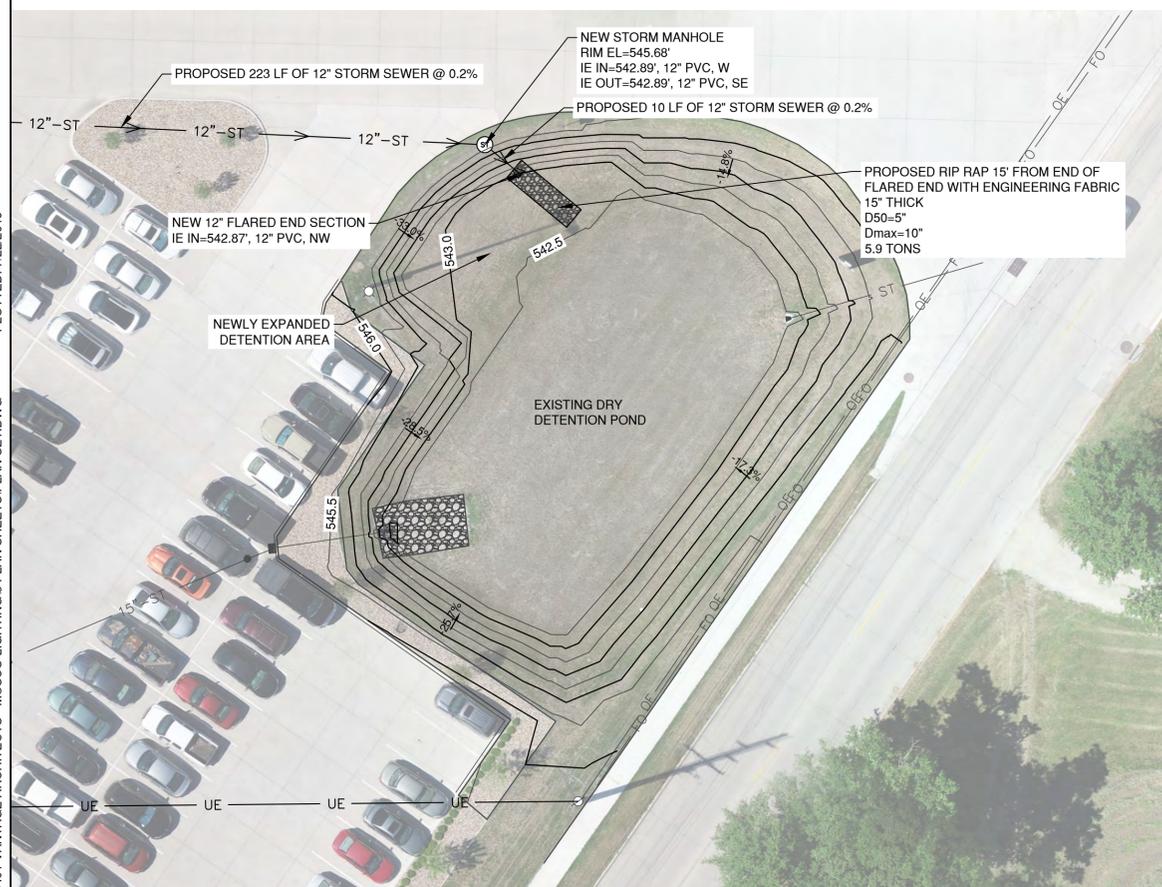
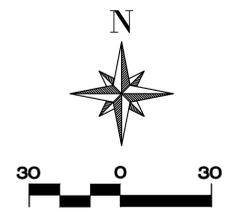
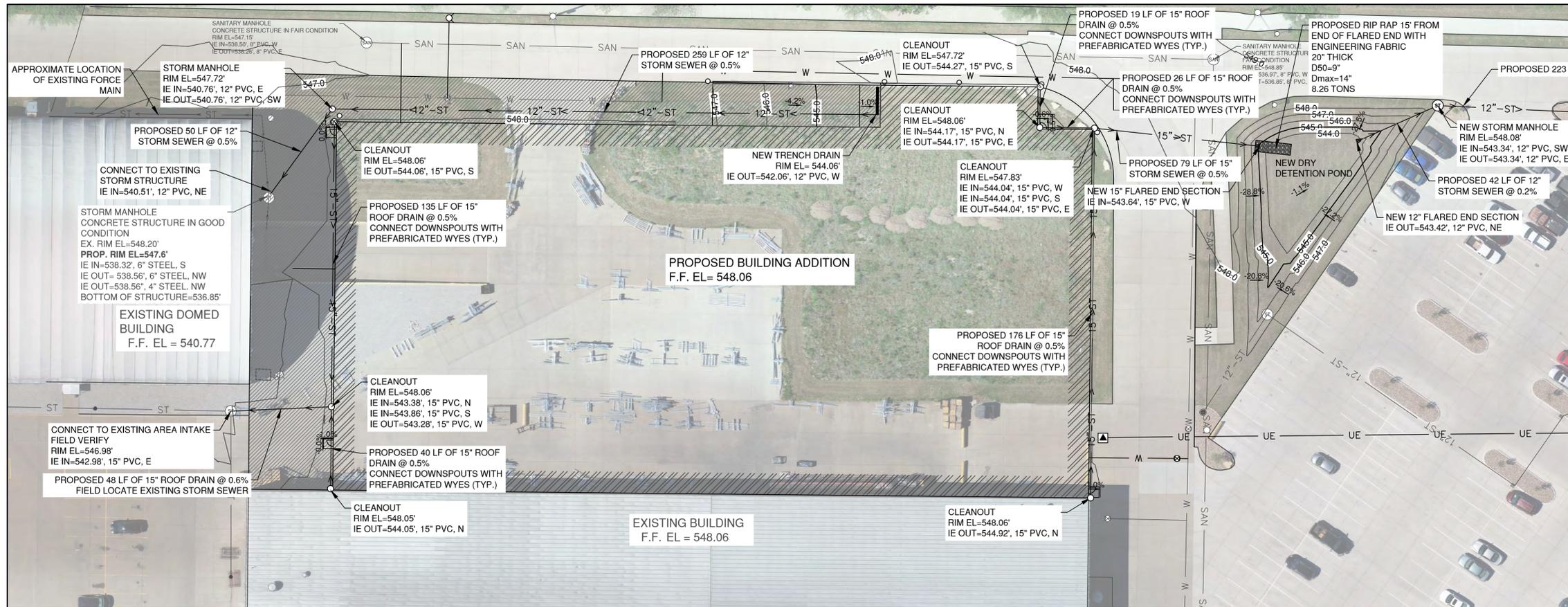


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DEMOLITION PLAN

PROJ NO:	8401.19	DRAWING NO:	F.01
CONST PROJ:	000		
SCALE:	AS NOTED	SHEET NO:	4 OF 8
DATE:	7/19/2019		



DETENTION SUMMARY:

EXISTING DETENTION VOLUME	16,534 CF
ADDITIONAL REQUIRED DETENTION VOLUME	5,617 CF
NEW DETENTION POND	2,423 CF
NEW CONNECTION SEWER	362 CF
REVISED NE DETENTION POND	3,071 CF
PROVIDED DETENTION VOLUME	5,856 CF

EXISTING OUTLET PIPE ELEVATION 542.36' 12" HDPE STORM SEWER

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		DRAWN BY:	JMS
		PROJECT ENGR:	MWJ

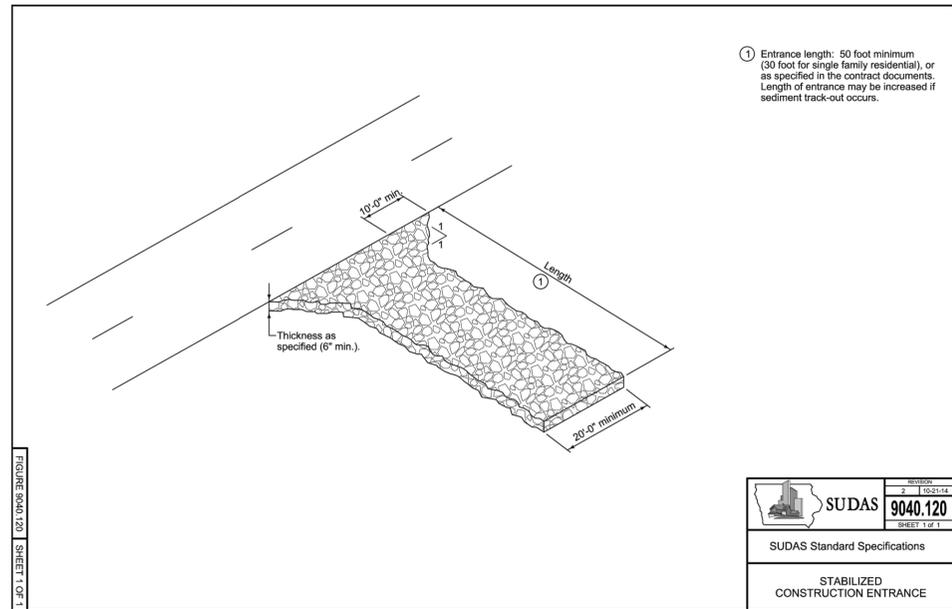
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BUILDING ADDITION
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STORM SEWER & DETENTION PLAN

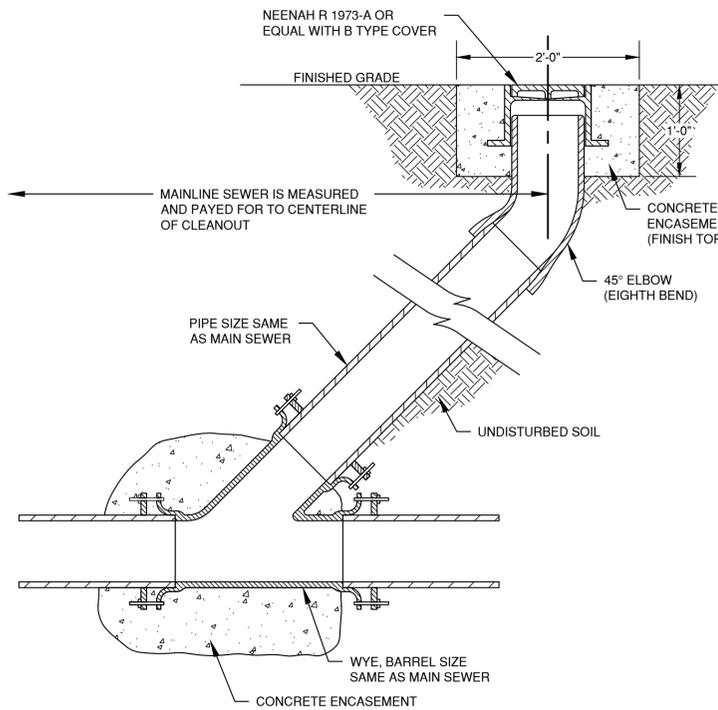
PROJ NO:	8401.19	DRAWING NO:	M.01
CONST PROJ:	000		
SCALE:	AS NOTED	SHEET NO:	6 OF 8
DATE:	7/19/2019		

FIGURE 9040.120 SHEET 1 OF 1

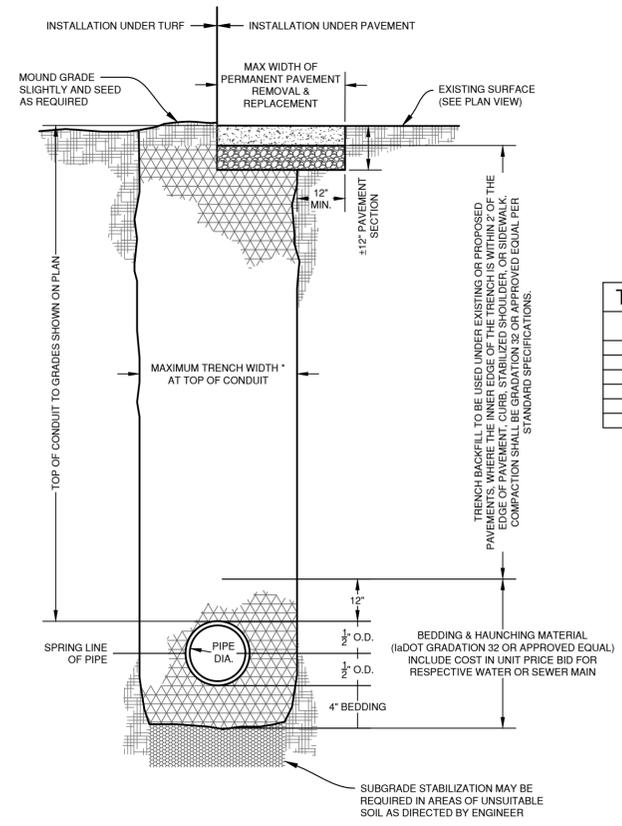


	9040.120
	SHEET 1 OF 1
SUDAS Standard Specifications	
STABILIZED CONSTRUCTION ENTRANCE	

TEMPORARY CONSTRUCTION ENTRANCE
NOT TO SCALE

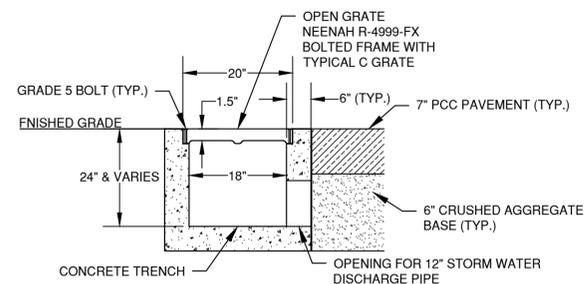


TYPICAL CLEANOUT INSTALLATION
NOT TO SCALE

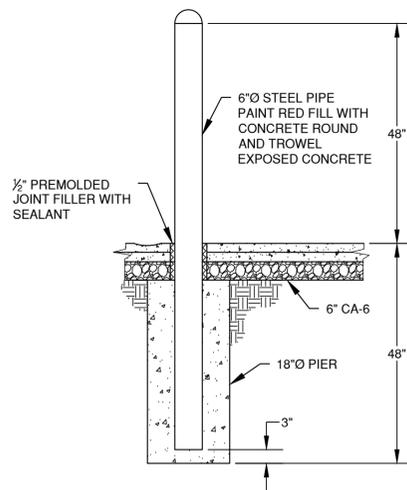


INSIDE DIA. (INCHES)	MAX TRENCH WIDTH (FT)
8	3.78
12	4.17
15	4.46
18	4.75
24	5.33
30	5.92

UTILITY TRENCH DETAIL
NOT TO SCALE



DRIVEWAY STORMWATER TRENCH DETAIL
NOT TO SCALE



BOLLARD DETAIL
NO SCALE

FENCING NOTES

1. MAXIMUM POST SPACING SHALL BE 10' C-C.
2. PROVIDE PULL POST AS REQUIRED FOR PROPER STRETCHING OF FABRIC.
3. MINIMUM CLEARANCE BETWEEN POST AND MONOLITHIC JOINT SHALL BE 6".
4. ALL PIPE DIAMETERS SHOWN ARE STANDARD STEEL PIPE SIZES.
5. ALL FENCING AND POSTS SHALL BE GALVANIZED.

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		DRAWN BY:	JMS
		PROJECT ENGR:	MWJ



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DETAILS

PROJ NO:	8401.19	DRAWING NO:	U.01
CONST PROJ:	000		
SCALE:	AS NOTED	SHEET NO:	8 OF 8
DATE:	7/19/2019		